

VILLAGE OF VERNON HILLS

ORDINANCE NO. 2014-015

AN ORDINANCE GRANTING APPROVAL TO AMEND THE EXISTING SPECIAL USE PERMIT AND CERTAIN OTHER APPROVALS TO ALLOW MCDONALDS TO EXPAND ITS DRIVE -THRU LANES AND REMODEL THE EXISTING RESTAURANT AT 700 NORTH MILWAUKEE AVENUE, UNIT 147 IN THE VILLAGE OF VERNON HILLS, LAKE COUNTY.

THE 19<sup>th</sup> DAY OF MARCH 2014

Published in pamphlet form by the Authority of the President and Board of Trustees of the Village of Vernon Hills, Lake County, Illinois, this 1<sup>st</sup> Day of April, 2014

**ORDINANCE NO. 2014-015**

**AN ORDINANCE GRANTING APPROVAL TO AMEND THE EXISTING SPECIAL USE PERMIT AND CERTAIN OTHER APPROVALS TO ALLOW MCDONALDS TO EXPAND ITS DRIVE -THRU LANES AND REMODEL THE EXISTING RESTAURANT AT 700 NORTH MILWAUKEE AVENUE, UNIT 147 IN THE VILLAGE OF VERNON HILLS, LAKE COUNTY.**

**WHEREAS**, Mary G. Meyers, McDonald's USA, LLC, representing the owners McDonald Corporation, for property located at 700 North Milwaukee Avenue, Unit 147 and legally described in Exhibit A, has petitioned the Village of Vernon Hills for the following:

1. To amend the existing special use permit and approved site and landscaping plans for the property to provide for the construction of a new side-by-side drive-thru facility, new exterior and interior improvements for the restaurant building; and
2. To approve, as a part of the Special Use Permit, certain variations including but not limited to signage, setbacks, buffers and parking/loading requirements; and,
3. Preliminary and final approval of the proposed site and landscaping plans; and,
4. Preliminary and final approval of the proposed architectural elevations and signage plans.

**WHEREAS**, Mary G. Meyers, McDonald's USA, LLC, requested the following variations as a part of the petition:

1. To allow a logo that will be 10.75 sq ft in lieu of the maximum size of 4 sq ft on the West Elevation.
2. For the North Elevation sign to allow two wall signs, a sign and a logo, in lieu of the maximum permitted number of one and to allow a logo that will be 10.75 sq ft in lieu of the maximum size of 4 square ft, which was amended by the Petitioner at the Village Board meeting to allow 1 wall sign, that will be 10.75 sq ft in lieu of the maximum size of 4 square feet.
3. To allow a double gateway directional sign on the island between the drive thru isles with 3.2 sq ft drive thru language above each arm and "Any Lane Any Time" sign not being a separate sign but on the post.
4. To allow two menu boards, one for each drive thru, that are each 38.8 sq ft, in lieu of the maximum size of 32 sq ft.

5. To allow the "Pull Forward" bollard sign listed as Detail 5, Option A on Page DT-4, dated 1/28/14.

**WHEREAS**, the requested approvals would be in general compliance with the following plans:

- Proposed Final Engineering Plans prepared by Watermark Engineering Resources, LTD with revised date of 1/28/14 and consisting of pages C-1 thru 6.
- Proposed Drive-Thru pavement Plans prepared by Watermark Engineering Resources, LTD with revised date of 1/28/14 and consisting of pages DT-1 thru 4.
- Proposed Drive-Thru Canopy and Menu Board Foundation Plans prepared by Samartano and Company with revised date of 5/30/13 and consisting of pages SSD-1 thru 2
- Landscape Plan prepared by Watermark Engineering Resources, LTD with a revised date of 1/28/14 and consisting of pages L-1 thru 2.
- Preliminary Elevations and Color Elevations/Material Board as found in the McDonald's Remodel packet with a stamped receive date of January 13, 2014 and Floor Plan (11/21/13 – A1.0) prepared by Lingle Design Group consisting of pages SD-1 thru SD-4.
- Signage Exhibit prepared by Watermark Engineering Resources, LTD and consisting of 1 page and dated 1/30/14.

Said Plans are attached hereto as Exhibit B; and,

**WHEREAS**, upon due notice and after public hearing held January 29, 2014 and continued from time to time by the Planning and Zoning Commission of the Village of Vernon Hills, and pursuant to the Vernon Hills Zoning Ordinance of 1982, as amended, said Planning and Zoning Commission has filed its report concerning said petition as listed above.

**WHEREAS**, based upon the evidence adduced at said hearings and in their application, the petitioner has entered into the record evidence and findings of fact that addresses the conditions in Section 18.3 of the Zoning Ordinance.

**NOW, THEREFORE, BE IT ORDAINED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF VERNON HILLS, COUNTY OF LAKE AND STATE OF ILLINOIS:**

**SECTION I.** Pursuant to the Vernon Hills Zoning Ordinance of 1982, as amended, the amendment to the existing Special Use Permit and the approved site and landscaping plans for the property to allow the construction of a new side-by-side drive thru facility, new exterior and interior improvements for the restaurant building pursuant to the plans listed in Exhibit B, is hereby granted. Said parcel is legally described in Exhibit A. The approval is subject to the conditions listed in Section VI and the terms and conditions as set forth in Exhibit C.

**SECTION II.** Pursuant to the Vernon Hills Zoning Ordinance of 1982, as amended, and Chapter 19 Signs, the List of Variations from Chapter 19 (Signs) of the Village Code as set forth in Exhibit B is hereby granted as amended and subject to the conditions listed in Section VI.

**SECTION III.** Pursuant to the Vernon Hills Zoning Ordinance of 1982, approval of the preliminary and final site and landscaping plans for the entire site, including the free standing signage exhibit dated 1/30/2014 with the amendment to use the double welcome point gateway directional sign to the drive-thru as set forth in Exhibit B are hereby granted subject to the conditions listed in Section VI.

**SECTION IV.** Pursuant to the Vernon Hills Zoning Ordinance of 1982, the preliminary and final approvals of the architectural elevations (Preliminary Elevations and Color Elevations/Material Sample Board) including signage but excluding the logo on the north elevation as set forth in Exhibit B are hereby granted subject to the conditions listed in Section VI.

**SECTION V.** Pursuant to the Vernon Hills Zoning Ordinance of 1982, the preliminary and final site improvement plans (as illustrated in the Engineering and Drive-Thru Pavement Plans) as set forth in Exhibit B are hereby approved subject to the conditions listed below in Section VI.

**SECTION VI.** Pursuant to the Vernon Hills Zoning Ordinance of 1982, as amended, and the recommendations of the Planning and Zoning Commission, the terms and conditions as set forth in Exhibit C are hereby approved and are made a part of the approvals as listed in the Sections above.

**SECTION VII. SEVERABILITY.** In the event that any section, clause, provision, or part of this ordinance shall be found and determined to be invalid by a court of competent jurisdiction, all valid parts that are severable from the invalid parts shall remain in full force and effect. If any part of this ordinance is found to be invalid in any one or more of its several applications, all valid applications that are severable from the invalid applications shall remain in effect.

**SECTION VIII. REPEAL AND SAVINGS CLAUSE.** All ordinances or parts of ordinances in conflict herewith are hereby repealed; provided, however, that nothing herein contained shall affect any rights, actions or causes of action which shall have accrued to the Village of Vernon Hills prior to the effective date of this ordinance.

**SECTION IX. SUCCESSORS AND ASSIGNS.** All of the provisions of this Ordinance and the attachments hereto are binding on all successors and assigns of the petitioner and property owner.

**SECTION X. EFFECTIVE DATE.** This ordinance shall be in full force and effect from and after its passage, approval and publication as provided by law.

**SECTION XI. ORDINANCE NUMBER.** This ordinance shall be known as Ordinance Number 2014-015.

Adopted by roll call vote as follows:

AYES: 6 – Schultz, Koch, Marquardt, Williams, Hebda, Byrne

NAYS: 0 - None

ABSENT AND NOT VOTING: 1 - -Schwartz

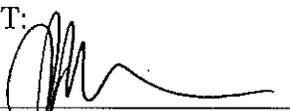
  
\_\_\_\_\_  
Roger L. Byrne  
Village President

PASSED: 3/19/2014

APPROVED: 3/19/2014

PUBLISHED IN PAMPHLET FORM: 4/01/2014

ATTEST:

  
\_\_\_\_\_  
John M. Kalmar  
Village Clerk

**Exhibit A**  
**Legal Description**

**MCDONALD'S / 700 N. MILWAUKEE AVENUE, VERNON HILLS, ILLINOIS**

LOT 3 IN THE FINAL PLAT OF SUBDIVISION OF HAWTHORN HILLS FASHION SQUARE, BEING A SUBDIVISION OF PART OF THE NORTH WEST ¼ OF SECTION 3 AND THE NORTH EAST ¼ OF SECTION 4, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED AUGUST 1, 1986 AS DOCUMENT 2467230, IN LAKE COUNTY, ILLINOIS.

**Exhibit B  
Plans**

General compliance with the following plans:

- Proposed Final Engineering Plans prepared by Watermark Engineering Resources, LTD with revised date of 1/28/14 and consisting of pages C-1 thru 6.
- Proposed Drive-Thru pavement Plans prepared by Watermark Engineering Resources, LTD with revised date of 1/28/14 and consisting of pages DT-1 thru 4.
- Proposed Drive-Thru Canopy and Menu Board Foundation Plans prepared by Samartano and Company with revised date of 5/30/13 and consisting of pages SSD-1 thru 2
- Landscape Plan prepared by Watermark Engineering Resources, LTD with a revised date of 1/28/14 and consisting of pages L-1 thru 2.
- Preliminary Elevations and Color Elevations/Material Sample Board as found in the McDonald's Remodel packet with a stamped receive date of January 13, 2014 and Floor Plan (11/21/13 – A1.0) prepared by Lingle Design Group consisting of pages SD-1 thru SD-4.
- Signage Exhibit prepared by Watermark Engineering Resources, LTD and consisting of 1 page and dated 3/21/14.

**Exhibit C**  
**Terms and Conditions of Approval**

List of Approved Variations from Chapter 19 (Signs) of the Village Code

1. To allow a logo "golden arches" that will be 10.75 sq ft in lieu of the maximum size of 4 sq ft on the West and North (front) Elevations.
2. To allow a double gateway directional sign on the island between the drive thru isles with 3.2 sq ft drive thru language above each arm and "Any Lane Any Time" sign not being a separate sign but on the post.
3. To allow two menu boards, one for each drive thru, that are each 38.8 sq ft, in lieu of the maximum size of 32 sq ft.
4. To allow the "Pull Forward" bollard sign listed as Detail 5, Option A on Page DT-4, dated 1/28/14.

Conditions of Approval for the entire Site:

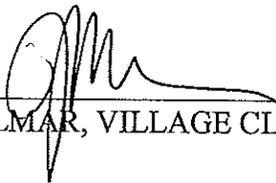
1. Final approval of plans by the Village Engineer and Landscape Technician prior to issuance of any site development or building permits.
2. The Building Commissioner's shall approve the ADA elements and the proposed lighting systems prior to issuance of a building permit. The current lighting system does not meet Village Code on glare and horizontal cut-offs. The petitioner shall provide the necessary information including a Photometric Plan for the property and upgrade the lighting system to meet the Village Code as may be required. Please note that this may require the addition of more light poles within the parking lot to comply with the Village Code requirements.
3. The petitioner shall provide all necessary written documentation and supporting information as required by the Village Engineer/Director of Public Works to verify compliance with the Countywide Watershed Development Ordinance regarding "Grandfathering" of the project from Ordinance requirements.
4. The final engineering plans shall be updated to include information as required by the Village Engineer/Director of Public works including but not limited to information related to the storm sewer system to clarified/verify the normal and customary direction of flow and the connection points within the system.
5. Compliance with the material sample board that will be provided by the petitioner at the public hearing.
6. Compliance with all ordinances and standards of the Village except as otherwise provided for herein.
7. The welcome sign above the west elevation will be facing north towards the parking lot.
8. Compliance with the free standing signage plans dated 1/30/2014 as amended
9. On the North (front) elevation, the word McDonalds shall be eliminated.



AFFIDAVIT OF SERVICE

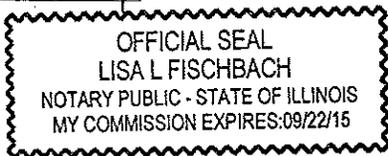
STATE OF ILLINOIS     )  
  )  
COUNTY OF LAKE        )

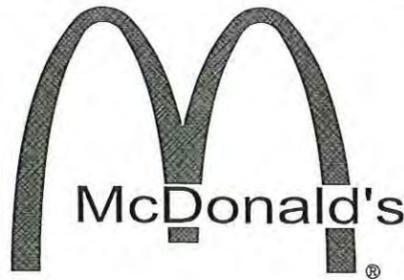
I, JOHN M. KALMAR, BEING FIRST DULY APPOINTED, DEPOSES AND SAYS ON OATH THAT AS VILLAGE CLERK OF THE VILLAGE OF VERNON HILLS, HE DID CAUSE THE FOREGOING CERTIFICATE FOR ORDINANCE 2014-015, AN ORDINANCE GRANTING APPROVAL TO AMEND THE EXISTING SPECIAL USE PERMIT AND CERTAIN OTHER APPROVALS TO ALLOW MCDONALDS TO EXPAND ITS DRIVE - THRU LANES AND REMODEL THE EXISTING RESTAURANT AT 700 NORTH MILWAUKEE AVENUE, UNIT 147 IN THE VILLAGE OF VERNON HILLS, LAKE COUNTY TO BE POSTED IN THE VILLAGE HALL AS REQUIRED BY LAW FROM APRIL 1, 2014 TO APRIL 11, 2014.

  
\_\_\_\_\_  
JOHN M. KALMAR, VILLAGE CLERK

SUBSCRIBED AND SWORN TO BEFORE  
THIS 1<sup>st</sup> DAY OF APRIL, 2014

  
\_\_\_\_\_  
Notary Public





# McDonald's - VERNON HILLS, IL

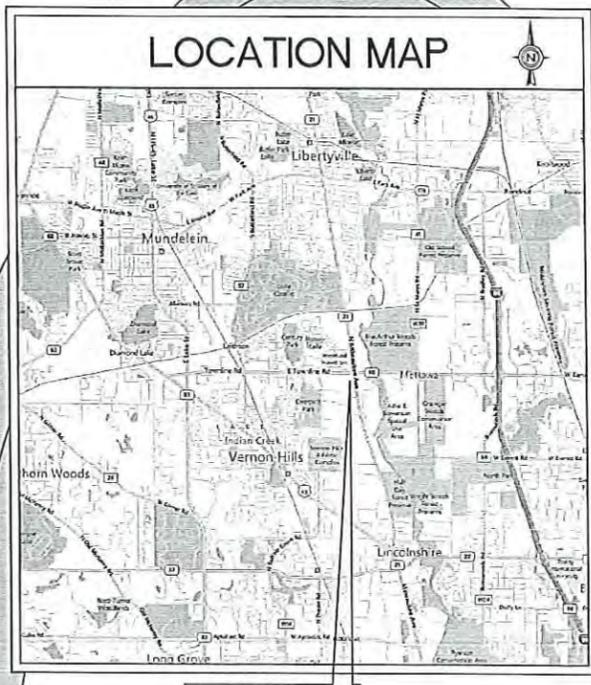
## FINAL ENGINEERING PLANS

FOR

AT  
700 N. MILWAUKEE AVENUE  
VILLAGE OF VERNON HILLS  
LAKE COUNTY, ILLINOIS

ORDINANCE 2014-015, Exhibit B (1) Proposed Final  
Engineering Plans prepared by Watermark Engineering  
Resources, LTD with revised date of 1/28/14 and consisting of  
pages C-1 thru 6.

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C-6	PROJECT SPECIFICATIONS
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L-1	LANDSCAPE PLAN
L-2	EX. CONDITIONS/ DETAILS AND SPECS
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DT-1	DRIVE-THRU PAVEMENT PLAN
DT-2	DRIVE-THRU EQUIPMENT PLAN
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DT-4	DRIVE-THRU DETAILS
SSD-1	SITE STRUCTURAL DETAILS
SSD-2	SITE STRUCTURAL DETAILS



SITE LOCATION

LEGEND		
EXISTING	DESCRIPTION	PROPOSED
⊙	CATCH BASIN	⊙
○	INLET	○
⊙	STORM MANHOLE	⊙
⊙	SANITARY MANHOLE	⊙
⊙	VALVE VAULT	⊙
⊙	FIRE HYDRANT	⊙
⊙	FLARED END SECTION	⊙
⊙	ELECTRICAL POWER POLE	⊙
⊙	OVERHEAD TRAFFIC SIGNAL	⊙
⊙	TRAFFIC SIGNAL MANHOLE	⊙
⊙	OVERHEAD ELECTRIC WIRES	⊙
⊙	TRANSFORMER PAD	⊙
⊙	TELEPHONE PEDESTAL	⊙
⊙	TELEPHONE MANHOLE	⊙
⊙	CABLE TELEVISION PEDESTAL	⊙
⊙	COMMONWEALTH EDISON MANHOLE	⊙
⊙	B/BOX	⊙
⊙	LIGHT POLE	⊙
⊙	SIGN	⊙
⊙	BOLLARD POLE	⊙
⊙	GAS MARKER	⊙
⊙	ELECTRIC MARKER	⊙
⊙	TELEPHONE MARKER	⊙
⊙	WATER MAIN	⊙
⊙	GAS MAIN	⊙
⊙	ELECTRIC LINE	⊙
⊙	TELEPHONE LINE	⊙
⊙	CABLE TV LINE	⊙
⊙	SANITARY SEWER	⊙
⊙	STORM SEWER	⊙
⊙	GUY POLE	⊙
⊙	CONIFEROUS TREE W/DIAMETER	⊙
⊙	DECIDUOUS TREE W/DIAMETER	⊙
⊙	WOOD FENCE	⊙
⊙	CHAIN LINK FENCE	⊙
⊙	METAL GUARDRAIL	⊙
⊙	CONCRETE SURFACE	⊙
⊙	CONTOUR LINE	⊙
⊙	PAVEMENT ELEVATION	⊙
⊙	GROUND ELEVATION	⊙
⊙	TOP OF WALK ELEVATION	⊙
⊙	TOP OF RETAINING WALL ELEVATION	⊙
⊙	FLOW LINE ELEVATION	⊙
⊙	TOP OF CURB ELEVATION	⊙
⊙	FSM ELEVATION	⊙
⊙	DOWNSPOUT LOCATION	⊙
⊙	PERVIOUS AREA SLOPE DIRECTION	⊙
⊙	PAVEMENT SLOPE DIRECTION	⊙
⊙	OVERLAND OVERFLOW DIRECTION	⊙
⊙	INLET PROTECTION	⊙
⊙	INLET BASKET FILTER	⊙

BENCHMARK	
<b>REFERENCE BENCHMARK</b>	
VILLAGE OF VERNON HILLS STATION NO. 218 (VILLAGE OF VERNON HILLS DATUM) #5 REBAR WITH RED GPS CAP FLUSH WITH THE GROUND AT THE SOUTHEAST POINT ON EAST END OF SPIRAL SIDEWALK LOCATED AT THE SOUTHEAST CORNER OF TOWNLINE ROAD (U.S. ROUTE 60) AND MILWAUKEE AVENUE (U.S. ROUTE 21).	
	ELEVATION = 673.10
<b>SITE BENCHMARK</b>	
1. TAG BOLT OF FIRE HYDRANT LOCATED AT THE SOUTHWEST CORNER OF MILWAUKEE AVENUE AND TOWNLINE ROAD.	ELEVATION = 676.46
2. NORTHWEST BOLT OF FIRE HYDRANT LOCATED IN ISLAND NORTH OF BUILDING.	ELEVATION = 677.41

RECEIVED

JAN 29 2014

COMMUNITY DEVELOPMENT

STATE OF ILLINOIS) DEPARTMENT  
COUNTY OF KANE) SS

I, JEFFREY C. MILLER, A LICENSED PROFESSIONAL ENGINEER OF ILLINOIS, HEREBY  
CERTIFY THAT THESE CIVIL ENGINEERING PLANS, NOT THE SUPPORTING DOCUMENTS,  
AS LISTED IN THE INDEX, HAVE BEEN PREPARED BY WATERMARK ENGINEERING  
RESOURCES, LTD. UNDER MY PERSONAL DIRECTION. THESE PLANS ARE INTENDED  
TO BE USED AS AN INTEGRAL PART OF THE PROJECT SPECIFICATIONS AND  
CONTRACT DOCUMENTS.

DATE:  
ILLINOIS LICENSED PROFESSIONAL ENGINEER NO. 62-045558.  
MY LICENSE EXPIRES ON 11-30-15.

UNLESS THIS DOCUMENT BEARS ORIGINAL SIGNATURE AND EMBOSSED SEAL OF THE  
DESIGN ENGINEER, IT IS NOT A VALID DOCUMENT.  
ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184.002669

COVER SHEET

DATE	01/28/14
DRAWING	ALL
REVISIONS	REV 1 - 02/14
NO.	1
NO.	2
NO.	3
NO.	4
NO.	5
NO.	6
NO.	7
NO.	8
NO.	9
NO.	10
NO.	11
NO.	12
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NO.	93
NO.	94
NO.	95
NO.	96
NO.	97
NO.	98
NO.	99
NO.	100

Prepared For:  
Lingle Design Group  
158 W. Main Street  
Lena, IL 61048  
McDONALD'S - VERNON HILLS, IL  
700 N. Milwaukee Avenue  
Vernon Hills, Illinois

Prepared By:  
Watermark Engineering Resources, LTD  
2021 Client: Vernon Hills, IL  
phone 630-375-1800 fax 630-208-0800 www.watermark-engineering.com

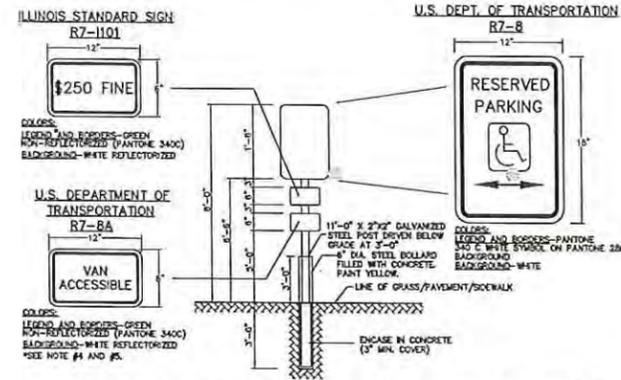
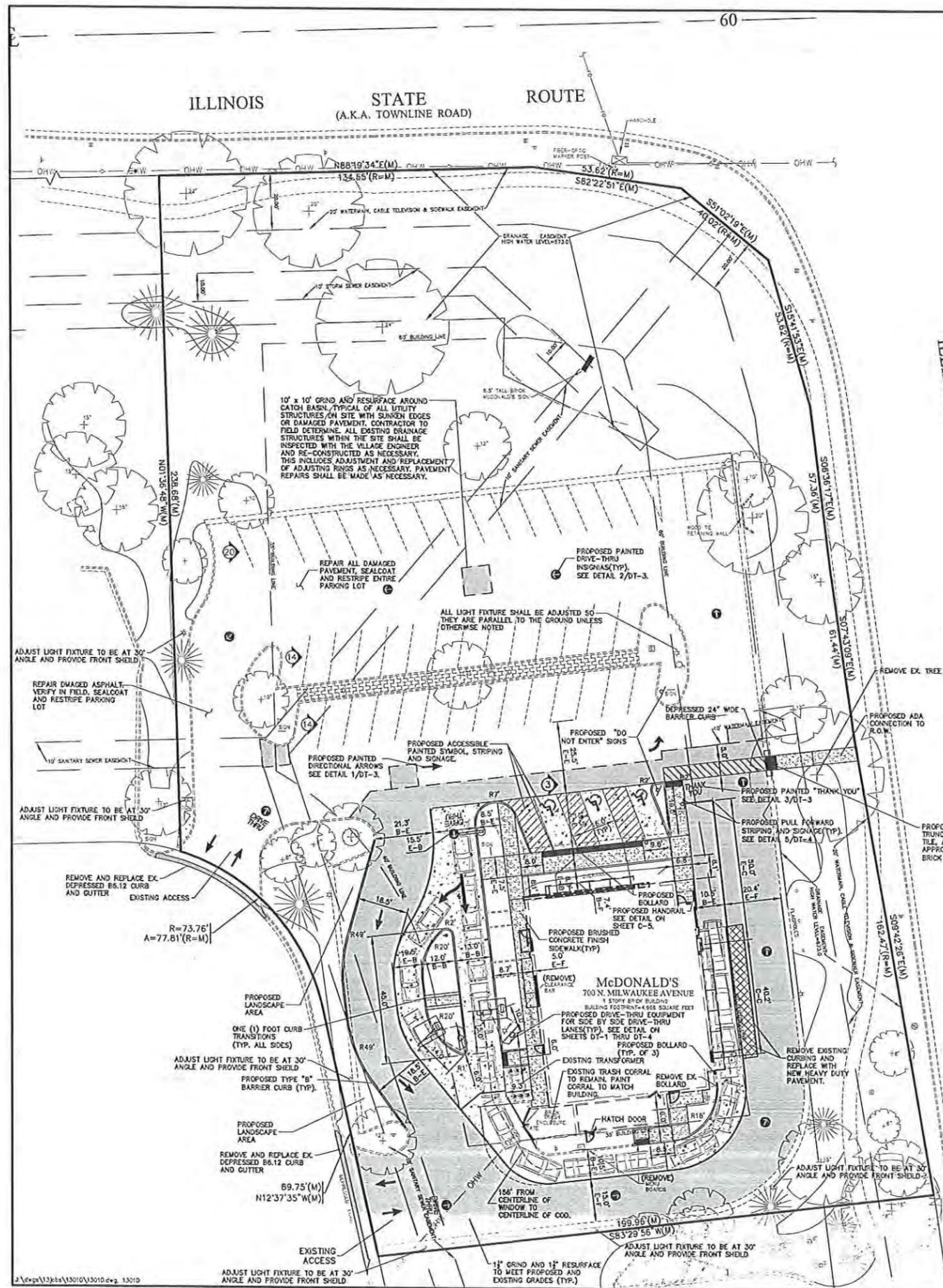
CHECKED BY: J. MILLER  
DESIGN BY: M. BAILEY  
DRAWN BY: K. SACK  
DATE: NOVEMBER 21, 2013  
SCALE: NONE  
PROJECT NO.: 13-010

C-1  
LC #12-1085

COVER SHEET

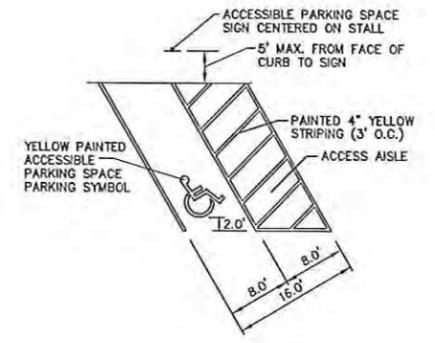


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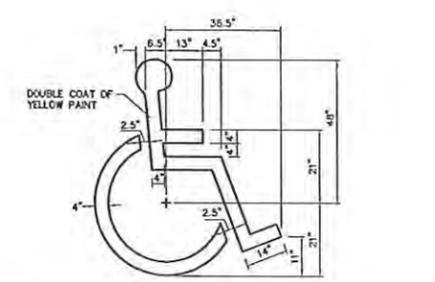


- WHERE A FINE IN EXCESS OF \$250 IS ESTABLISHED BY A MUNICIPALITY BY ORDINANCE IN ACCORDANCE WITH THE STATUTES, THE ACTUAL AMOUNT OF THE FINE SHOULD BE SHOWN.
- THIS PLATE MAY BE MOUNTED DIRECTLY BELOW THE R7-8 SIGN OR COMBINED WITH THAT SIGN ON A SINGLE 12 INCH BY 24 INCH PANEL.
- ON THE RESERVED PARKING SIGN, THE ARROW SHOULD BE OMITTED WHERE THERE IS ONLY ONE SPACE. THE ARROW MAY ALSO BE MADE TO POINT IN ONLY ONE DIRECTION. THE ARROW MAY ALSO BE REPLACED BY "TIME" SUCH AS 8 AM-5 PM WHERE A PART OF THE RESTRICTION EXISTS.
- ONE IN EVERY EIGHT ACCESSIBLE SPACES, BUT NOT LESS THAN ONE, SHALL BE SERVED BY AN ACCESS AISLE 8 FEET WIDE MINIMUM AND SHALL BE DESIGNATED "VAN ACCESSIBLE".
- THE LOWEST BOTTOM EDGE OF THE LOWEST REQUIRED SIGN SHALL BE MOUNTED AT 60" ABOVE FINISHED GRADE.

ACCESSIBLE PARKING SPACE SIGN WITH BOLLARD DETAIL



ACCESSIBLE PARKING SPACE DETAIL



- NOTES:
- SYMBOL IS CENTERED ON WIDTH OF PARKING STALL AND 2' FROM THE END OF THE STALL.

ACCESSIBLE PARKING SPACE PARKING SYMBOL

**GENERAL NOTES:**

- THESE PLANS ARE BASED ON THE BOUNDARY AND TOPOGRAPHIC SURVEY (SURVEY PROJECT #13.0016 DATED 01/31/13) PREPARED BY: COMPASS SURVEYING LTD 2631 GINGER WOODS PARKWAY, STE 100, AURORA, IL 60502 (630) 820-9100
- PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.
- NO PROPOSED MODIFICATION TO SITE LIGHTING.
- PER FIRE CODE REQUIREMENTS ACCESS MUST BE MAINTAINED TO THE BUILDING DURING THE CONSTRUCTION.

**GEOMETRIC PLAN NOTES:**

- ALL RADIUS DIMENSIONS ARE TO BACK OF CURB.
- SEE ARCH. PLANS FOR EXACT BUILDING DIMENSIONS.
- ALL STRIPING TO BE DOUBLE COATED 4" WHITE PAINT UNLESS OTHERWISE NOTED.

**SITE DATA**

LOT AREA =	82,995 S.F.(1.90 AC.)
EXISTING BUILDING AREA =	4,776 S.F.
EXISTING BUILDING SEATING =	80 SEATS
EMPLOYEES PER SHIFT =	12 EMPLOYEES
F.A.R. =	0.06

**EXISTING PAVEMENT DATA**

EXISTING PAVEMENT AREA =	37,647 S.F.(0.86 AC.)(45.3%)
EXISTING IMPERVIOUS AREA =	45,348 S.F.(1.04 AC.)(54.7%)
PROPOSED PAVEMENT AREA =	38,098 S.F.(0.87 AC.)(45.5%)
PROPOSED IMPERVIOUS AREA =	44,897 S.F.(1.03 AC.)(54.5%)

NOTE: NO CHANGES TO THE EXISTING DETENTION BASIN VOLUME ARE PROPOSED AS PART OF THIS PROJECT.

**ON SITE PARKING DATA**

EXISTING REGULAR SPACES	59
EXISTING ADA ACCESSIBLE SPACES PROVIDED	2
TOTAL EXISTING SPACES	61
PROPOSED REGULAR SPACES	48
PROPOSED ADA ACCESSIBLE SPACES PROVIDED	3
TOTAL PROPOSED SPACES	51

TOTAL SPACES REQUIRED = 1 SPACE/2 SEATS + 1 SPACE/2 EMPLOYEES.  
 TOTAL SPACES REQUIRED = 80/2 + 12/2 = 46  
 51 PROPOSED SPACES > 46 REQUIRED SPACES

**DRIVE THRU STACKING DATA**  
 NOT REQUIRED

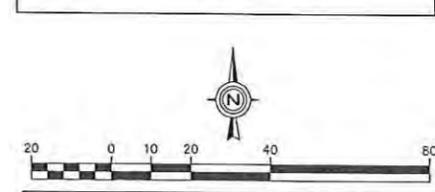
**BICYCLE PARKING DATA**  
 NOT REQUIRED

**McDONALD'S PAVEMENT LEGEND**

SIDEWALK	5" P.C.C. (6X BAG MAX) 4" AGGREGATE BASE COURSE (CA-6) COMPACTED SUBGRADE
HEAVY DUTY	1 1/2" HMA SURFACE COURSE 1/2" N50 3" HMA BINDER COURSE N50
CONCRETE	12" CRUSHED LIMESTONE AGGREGATE COURSE (CA-6) COMPACTED SUBGRADE 4 OZ WOVEN FABRIC (CLAY SUB-BASE) OR 8 OZ NON-WOVEN FABRIC (SANDY/GRANULAR SUB-BASE)
	6" P.C.C. WITH 6"x6" W2.9 W2.9 WELDED WIRE MESH 4" AGGREGATE BASE COURSE (CA-6) COMPACTED SUBGRADE 4 OZ WOVEN FABRIC (CLAY SUB-BASE) OR 8 OZ NON-WOVEN FABRIC (SANDY/GRANULAR SUB-BASE)

GRIND 1 1/2" AND RESURFACE 1 1/2" MIN. TO MATCH EXISTING AND PROPOSED ELEVATIONS

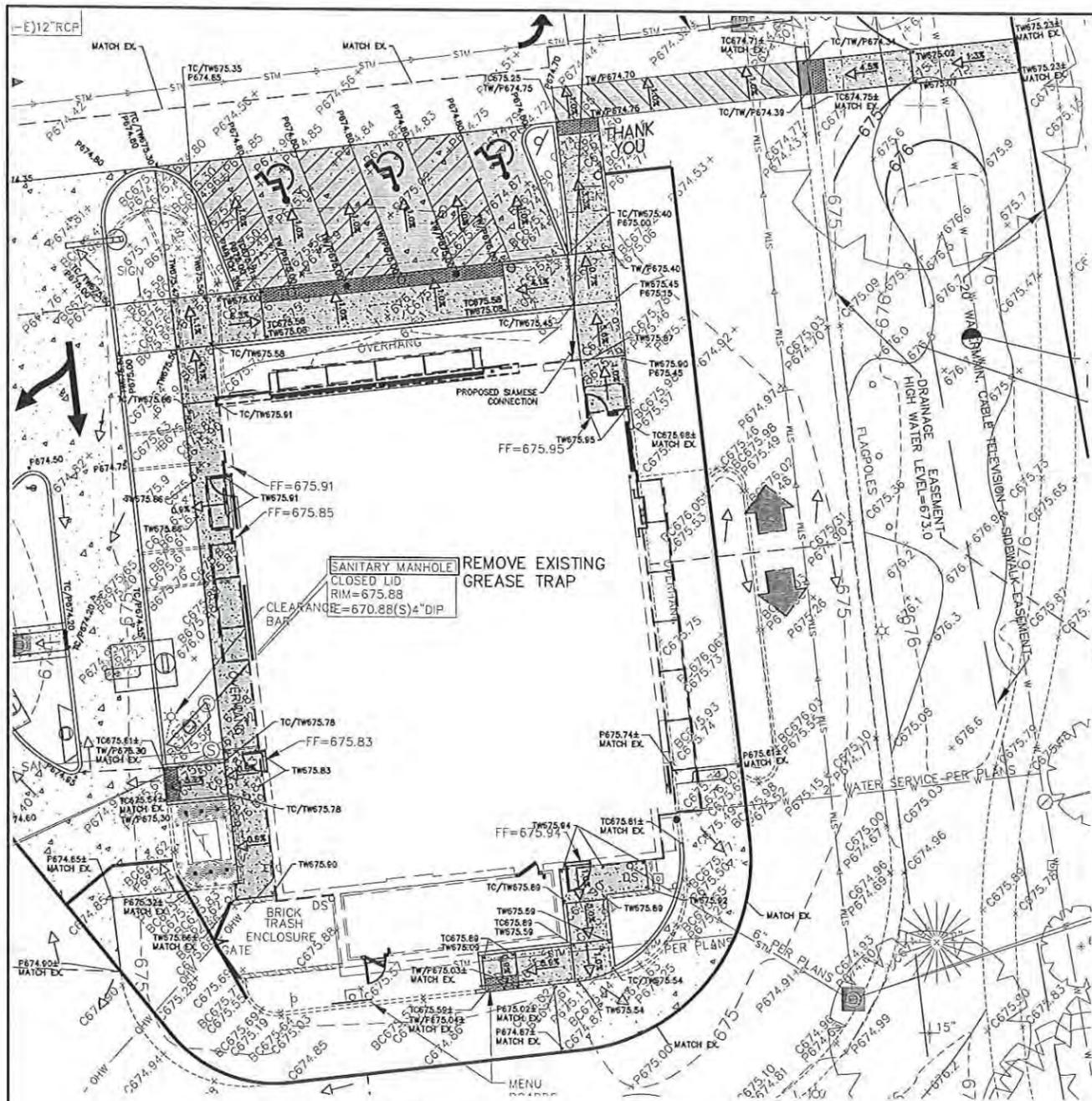
- NOTES:**
- REFERENCE I.D.O.T. STANDARD SPECIFICATIONS (LATEST EDITION) SECTION 406 FOR BINDER & SURFACE COURSES, SECTION 351 FOR AGGREGATE BASE COURSE, AND SECTION 1080 FOR FABRIC MATERIAL.
  - THE APPLICATION RATES FOR THE PRIME COAT AND TACK COAT ARE TO BE 0.30 AND 0.10 GALLONS PER SQUARE YARD, RESPECTIVELY.
  - THE ENTIRE SUBGRADE SHALL BE COMPACTED TO NOT LESS THAN 95% OF THE STANDARD LABORATORY DENSITY PER SECTION 301.
  - PROVIDE CONTRACTION JOINTS ON CONCRETE PAVEMENT AT 18' O.C. FOR SIDEWALKS, PROVIDE CONTROL JOINT AT 5' O.C., EXPANSION JOINTS AT 50' O.C.



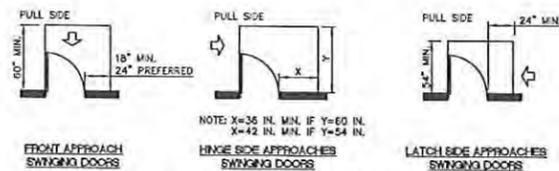
GEOMETRIC PLAN

DATE 01.09.14	REVISIONS REV 1 - BASE REVISION	NO.	1	2
Prepared For: <b>Lingle Design Group</b> 158 W. Main Street Lena, IL 61048				
<b>McDONALD'S - VERNON HILLS, IL</b> 700 N. Milwaukee Avenue Vernon Hills, Illinois				
Prepared By: <b>Watermark Engineering Resources, Ltd</b> 2631 GINGER WOODS PARKWAY, STE 100, AURORA, IL 60502 PHONE 630-371-5100 WWW.WATERMARK-ENGINEERING.COM				
CHECKED BY: J. MILLER	DESIGN BY: M. BAILEY	DATE: NOVEMBER 21, 2013	PROJECT NO.: 13-010	
DRAWN BY: K. SACK	SCALE: 1" = 20'	C-2		
LC #12-1085				



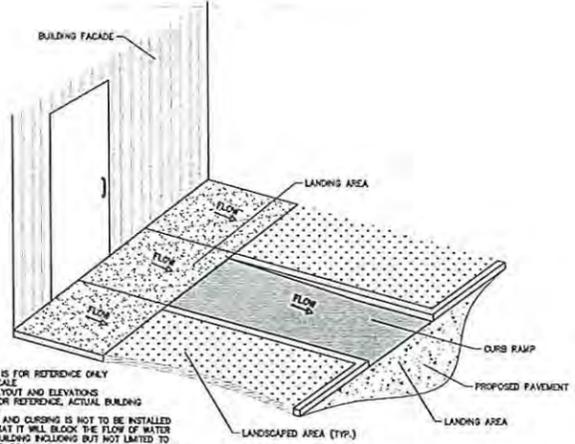


ACCESSIBLE ROUTE GRADES - PLAN VIEW SCALE: 1" = 10' - A.D.A. ACCESSIBLE ROUTE

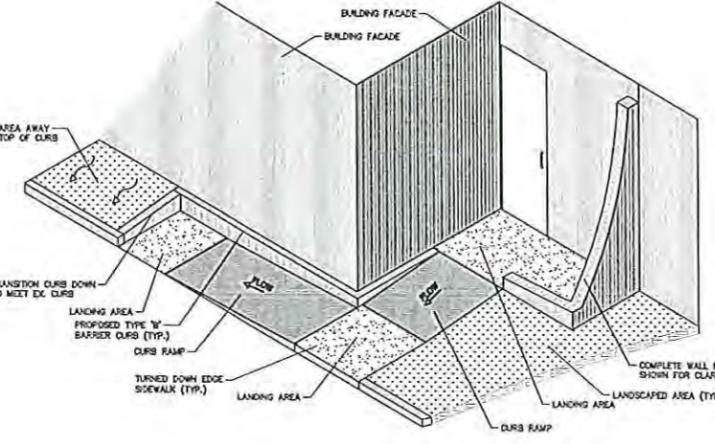


TYPICAL ACCESSIBLE LANDING AREA DETAIL

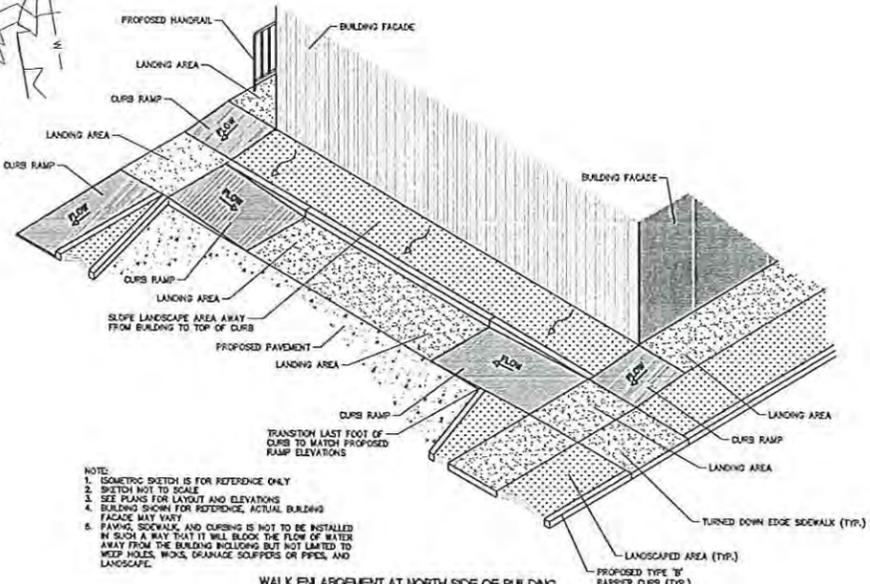
- NOTES:
1. ISOMETRIC SKETCH IS FOR REFERENCE ONLY
  2. SKETCH NOT TO SCALE
  3. SEE PLANS FOR LAYOUT AND ELEVATIONS
  4. BUILDING SHOWN FOR REFERENCE, ACTUAL BUILDING FACADE MAY VARY
  5. PAVING, SIDEWALK, AND CURBING IS NOT TO BE INSTALLED IN SUCH A WAY THAT IT WILL BLOCK THE FLOW OF WATER AWAY FROM THE BUILDING INCLUDING BUT NOT LIMITED TO WEEP HOLES, WOODS, DRAINAGE SCUPPERS OR PIPES, AND LANDSCAPE.



WALK ENLARGEMENT AT WEST SIDE OF BUILDING



WALK ENLARGEMENT AT SOUTH SIDE OF BUILDING



WALK ENLARGEMENT AT NORTH SIDE OF BUILDING

- NOTES:
1. ISOMETRIC SKETCH IS FOR REFERENCE ONLY
  2. SKETCH NOT TO SCALE
  3. SEE PLANS FOR LAYOUT AND ELEVATIONS
  4. BUILDING SHOWN FOR REFERENCE, ACTUAL BUILDING FACADE MAY VARY
  5. PAVING, SIDEWALK, AND CURBING IS NOT TO BE INSTALLED IN SUCH A WAY THAT IT WILL BLOCK THE FLOW OF WATER AWAY FROM THE BUILDING INCLUDING BUT NOT LIMITED TO WEEP HOLES, WOODS, DRAINAGE SCUPPERS OR PIPES, AND LANDSCAPE.

**GENERAL NOTES:**

1. THESE PLANS ARE BASED ON THE BOUNDARY AND TOPOGRAPHIC SURVEY (SURVEY PROJECT #13.0016 DATED 01/31/13) PREPARED BY: COMPASS SURVEYING LTD 2631 GINGER WOODS PARKWAY, STE 100, AURORA, IL 60502 (630) 820-9100
2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.
3. NO PROPOSED MODIFICATION TO SITE LIGHTING.

**REFERENCE BENCHMARK**  
VILLAGE OF VERNON HILLS STATION NO. 218 (VILLAGE OF VERNON HILLS DATUM)  
#5 REBAR WITH RED CAP FLUSH WITH THE GROUND AT THE SOUTHEAST POINT ON EAST END OF SPIRAL SIDEWALK LOCATED AT THE SOUTHEAST CORNER OF TOWNLINE ROAD (U.S. ROUTE 60) AND MILWAUKEE AVENUE (U.S. ROUTE 21).  
ELEVATION = 673.10

**SITE BENCHMARKS**

1. TAG BOLT OF FIRE HYDRANT LOCATED AT THE SOUTHWEST CORNER OF MILWAUKEE AVENUE AND TOWNLINE ROAD. ELEVATION = 676.46
2. NORTHWEST BOLT OF FIRE HYDRANT LOCATED IN ISLAND NORTH OF BUILDING. ELEVATION = 677.41

**GRADING PLAN NOTES:**

1. UNLESS OTHERWISE SPECIFIED, TOP OF CURB (TC) AND/OR TOP OF WALK ELEVATIONS ARE 0.5' HIGHER THAN THE ADJACENT FLOW LINE (FL) OR PAVEMENT (P) ELEVATIONS.
2. IN ALL LOCATIONS WHERE ELEVATIONS ARE SHOWN AS ±, THE ELEVATION HAS BEEN DETERMINED BASED ON INTERPOLATED GRADES FROM THE SURVEY. CONTRACTOR IS TO VERIFY THESE GRADES PRIOR TO CONSTRUCTION OF ANY IMPROVEMENTS WITH THE PROXIMITY OF THESE INTERPOLATED GRADES AND REPORT THEM TO THE DESIGN ENGINEER FOR VERIFICATION OF PROPOSED SLOPES PRIOR TO INSTALLATION OF PROPOSED IMPROVEMENTS. DESIGN ENGINEER IS NOT RESPONSIBLE FOR SLOPES OF PROPOSED IMPROVEMENTS BASED ON THESE ± GRADES WITHOUT CONFIRMATION OF EXISTING ELEVATIONS AT TIME OF CONSTRUCTION.
3. PAVING, SIDEWALK, AND CURBING IS NOT TO BE INSTALLED IN SUCH A WAY THAT IT WILL BLOCK THE FLOW OF WATER AWAY FROM THE BUILDING INCLUDING BUT NOT LIMITED TO WEEP HOLES, WICKS, DRAINAGE SCUPPERS OR PIPES, AND LANDSCAPING.

**"AMERICANS WITH DISABILITIES ACT" (ADA) MINIMAL REQUIREMENTS:**

1. GENERAL CONTRACTOR TO BECOME FAMILIAR WITH AND APPLY THE ADA MINIMAL REQUIREMENTS AND REPORT TO ARCHITECT/ENGINEER ANY DISCREPANCIES BEFORE CONSTRUCTION.
2. ACCESSIBLE ROUTES ON AN ACCESSIBLE SITE AND FOR ANY NEW SITE IMPROVEMENTS SHALL BE PROVIDED TO SERVE ALL ACCESSIBLE SPACES OR ELEMENTS.
3. THE MINIMUM CLEAR WIDTH OF AN ACCESSIBLE ROUTE PER CODE IS 36".
4. EACH ACCESSIBLE PARKING SPACE IS TO BE A MINIMUM OF 16' WIDE, CONSISTING OF A 96" WIDE ACCESS AISLE AND A 96" WIDE PARKING SPACE. (SEE DETAIL). THE ACCESS AISLE MAY BE ON EITHER THE DRIVER'S SIDE OR THE PASSENGER'S SIDE OF THE ACCESSIBLE SPACE. THIS APPLIES TO 45, 60, AND 80' PARKING.
5. ACCESSIBLE PARKING SPACES ARE TO BE LOCATED AS CLOSE TO THE BUILDING ENTRANCE AS POSSIBLE AND SHALL BE IDENTIFIED WITH A SIGN.
6. RAMPS MUST NOT EXTEND OUT FROM THE CURB INTO THE ACCESS AISLE OF ANY ACCESSIBLE PARKING SPACE.
7. TWO PARKING SPACES MAY NOT SHARE AN ACCESS AISLE.
8. AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 1:20 (5.00%) IS A RAMP AND SHALL COMPLY WITH THE RAMP REQUIREMENTS.
9. AN ACCESSIBLE ROUTE MAY CROSS OPEN PAVEMENT OR FOLLOW A RAMP AS REQUIRED BY SITE-SPECIFIC CONDITIONS. THE RUNNING SLOPE OF AN ACCESSIBLE ROUTE ACROSS OPEN PAVEMENT MUST NOT EXCEED 1:20 (5.00%), WITH A CROSS SLOPE NOT EXCEEDING 1:50 (2.00%). SLOPES EXCEEDING 1:20 (5.00%), BUT LESS THAN 1:12 (8.33%), CONSTITUTE RAMPS AND MUST CONFORM TO THE REQUIREMENTS FOR RAMP DESIGN (HANDRAILS, CURBS, LANDINGS, RISE AND RUN LIMITS, ETC.) AS DETAILED ON THE CIVIL AND ARCHITECTURAL PLANS. NO RAMP SHALL HAVE A RUNNING SLOPE EXCEEDING 1:12 (8.33%), NOR HAVE A CROSS SLOPE EXCEEDING 1:50 (2.00%).
10. THE GENERAL CONTRACTOR/CONTRACTOR SHALL MEASURE THE SUBGRADE AND ACROSS FORMS PRIOR TO INSTALLATION OF ASPHALT OR CONCRETE IMPROVEMENTS TO ASSURE THE FINAL IMPROVEMENTS WILL MEET THESE MINIMAL ADA REQUIREMENTS. ANY DISCREPANCIES SHALL BE REPORTED TO THE CIVIL ENGINEER PRIOR TO INSTALLATION OF THE IMPROVEMENTS.

**CURB RAMPS**

11. A CURB RAMP SHALL BE PROVIDED WHEREVER AN ACCESSIBLE ROUTE CROSSES A CURB.
12. CURB RAMPS HAVE A MAXIMUM RISE OF 6", MAXIMUM SLOPE OF 1:12 (8.33%) AND DO NOT REQUIRE HANDRAILS.
13. IF A CURB RAMP IS LOCATED WHERE PEDESTRIANS MUST WALK ACROSS THE RAMP, OR WHERE IT IS NOT PROTECTED BY HANDRAILS, OR GUARDRAILS, IT SHALL HAVE FLARED SIDES; THE MAXIMUM SLOPE OF THE FLARE SHALL BE 1:12 (8.33%).



**ACCESSIBLE ROUTE GRADES AND DETAILS**

DATE	01/08/14
REVISIONS	REV. 1 - DATE REVISION
NO.	1 - 2

Prepared For:

Lingle Design Group  
158 W. Main Street  
Lena, IL 61048

McDonald's - VERNON HILLS, IL  
700 N. Milwaukee Avenue  
Vernon Hills, Illinois

Prepared By:

**Watermark Engineering Resources, Ltd.**  
2631 Ginger Woods Parkway, Suite 100, Aurora, IL 60502  
phone 630-375-1900 fax 630-335-0860 www.watermark-engineering.com

CHECKED BY: J. MILLER	PROJECT NO.: 13-010
DESIGN BY: M. BAILEY	SCALE: 1" = 10'
DRAWN BY: K. SACK	DATE: NOVEMBER 21, 2013
DATE: NOVEMBER 21, 2013	PROJECT NO.: 13-010

C-4

LC #12-1085

ACCESSIBLE ROUTE GRADES AND DETAILS

**PROJECT SPECIFICATIONS**

- CONTRACTOR IS TO FOLLOW ALL ORDINANCES AND REQUIREMENTS OF THE STATE, COMMUNITY, LOCAL DISTRICTS AND THE ILLINOIS ACCESSIBILITY CODE (IAC). ALL PROPOSED IMPROVEMENTS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" AS WELL AS THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" CURRENT EDITIONS.
- THE CONTRACTOR SHALL INDEMNIFY WATERMARK ENGINEERING RESOURCES, LTD (THE ENGINEER), ARCHITECT AND OWNER, THEIR AGENTS, ETC., FROM ALL LIABILITY INVOLVED WITH THE CONSTRUCTION. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR CONDUCTING WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, SPECIFICATIONS, AND ALL GOVERNMENTAL AGENCIES HAVING JURISDICTION OVER THIS DEVELOPMENT.
- THE CONTRACTOR IS RESPONSIBLE TO OBTAIN ALL PERMITS THAT ARE REQUIRED BY THE LOCAL AGENCIES.
- PRIOR TO BID AND PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL INSPECT THE SITE TO VERIFY THAT THERE ARE NO DISCREPANCIES BETWEEN THE PLANS AND THE ACTUAL CONDITIONS AT THE SITE. IF ANY DISCREPANCIES ARE FOUND, AT ANY TIME BEFORE OR DURING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER IMMEDIATELY (BEFORE ANY ADDITIONAL IMPROVEMENTS ARE INSTALLED) IN ORDER TO OBTAIN WRITTEN CONFIRMATION BY THE DESIGN ENGINEER AS TO ANY REVISIONS THAT MAY NEED TO BE MADE TO THE PLANS.
- PRIOR TO CONSTRUCTION, CONTRACTOR IS TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER, ALL GOVERNMENTAL AGENCIES HAVING JURISDICTION, AND ALL UTILITY COMPANIES THAT MAY BE AFFECTED BY THE PROPOSED CONSTRUCTION 2 WORKING DAYS PRIOR TO THE START OF CONSTRUCTION TO ARRANGE APPROPRIATE CONSTRUCTION INSPECTIONS.
- THE MUNICIPALITY SHALL HAVE THE AUTHORITY TO INSPECT, APPROVE, AND REJECT THE CONSTRUCTION OF ANY IMPROVEMENTS.
- PRIOR TO CONSTRUCTION OF ANY IMPROVEMENTS, THE CONTRACTOR MUST CALL J.U.L.I.E. FOR THE LOCATION AND STAKING OF EXISTING UNDERGROUND UTILITIES (GAS, ELECTRIC, TELEPHONE) AT 1-800-892-0123, 48 HOURS PRIOR TO DIGGING. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING RECORD DRAWINGS PER THE MUNICIPALITY AND/OR ANY OTHER AGENCY REQUIREMENTS. ANY CHANGES TO THE DRAWINGS MUST BE REPORTED TO THE ENGINEER BEFORE WORK PROGRESSES.
- THE PROPOSED IMPROVEMENTS MUST BE CONSTRUCTED IN ACCORDANCE WITH THE ENGINEERING PLANS AS APPROVED BY THE MUNICIPALITY.
- ALL QUANTITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR AND ARE TO BE VERIFIED PRIOR TO CONSTRUCTION. IF DISCREPANCIES OCCUR, THE CONTRACTOR IS TO CONTACT THE DESIGN ENGINEER IMMEDIATELY AND NO WORK IS TO BE DONE UNTIL APPROVED BY THE DESIGN ENGINEER.
- ANY RESTORATION NEEDED BECAUSE OF CONSTRUCTION SHALL BE PROVIDED BY THE CONTRACTOR AT NO ADDITIONAL COST.
- TRENCH BACKFILL MATERIAL (CA-6) IS REQUIRED UNDER AND WITHIN TWO FEET (2') OF SIDEWALKS AND PAVED AREAS. THIS BACKFILL SHALL BE IN SIX INCH (6") LIFTS AND COMPACTED TO 95% STANDARD PROCTOR.
- CONTRACTOR IS TO PROVIDE ALL TEMPORARY SIGNAGE AS REQUIRED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND LOCAL MUNICIPALITIES.
- ALL EXISTING DRAIN TILES THAT ARE ENCOUNTERED ARE TO BE RESTORED TO THEIR ORIGINAL CONDITION OR REROUTED TO THE PROPOSED STORM SEWER SYSTEM.
- RESTORATION OF EXISTING RIGHT-OF-WAYS IS TO BE COMPLETED WITH FOUR INCH (4") MINIMUM TOPSOIL AND SALT TOLERANT SOD UNLESS OTHERWISE NOTED.
- THE WATER SYSTEM CANNOT BE SHUT DOWN WITHOUT CONSENT BY THE OWNER OF THE SYSTEM. ALL FRAME ADJUSTMENTS SHALL BE MADE WITH PRE-CAST CONCRETE RINGS CONFORMING TO ASTM C-39 AND CANNOT EXCEED TWELVE INCHES (12").
- FRAMES SHALL BE SET WITH EZ STIKS (OR EQUAL) MATERIAL TO PREVENT LEAKAGE. THE REINFORCED CONCRETE SECTIONS SHALL BE LAID IN MORTAR, SEALED WITH EXTERNAL SEALING BANDS, OR SEALED USING MASTIC JOINT SEALER. WHEN MASTIC JOINT SEALER IS USED, THE MATERIAL SHALL COMPLETELY FILL THE JOINT AFTER THE UNITS HAVE BEEN BROUGHT TOGETHER.
- STEPS IN STRUCTURES SHALL BE MADE OF COPOLYMER POLYPROPYLENE PLASTIC WITH CONTINUOUS ONE HALF INCH (1/2") GRADE SIXTY (60) STEEL REINFORCEMENT, STEP PSI-PF, AS MANUFACTURED BY M.A. INDUSTRIES, INC., OR APPROVED EQUAL. STEPS TO BE SPACED SIXTEEN INCHES (16") ON-CENTER.
- ALL INSTRUMENTS ARE TO BE PROPERLY CALIBRATED PRIOR TO CONSTRUCTION USE.
- ALL PARKING LOT LIGHT POLES ARE TO BE CONSTRUCTED AT THE INTERSECTION OF PARKING LOT STRIPING OR IN LANDSCAPE AREAS WITH A MINIMUM OF 2' CLEARANCE BETWEEN THE BACK OF CURB AND THE EDGE OF THE PARKING LOT LIGHT BASE UNLESS OTHERWISE SPECIFIED.
- GENERAL CONTRACTOR TO BECOME FAMILIAR WITH AND APPLY THE ADA MINIMAL REQUIREMENTS AND REPORT TO ARCHITECT/ENGINEER ANY DISCREPANCIES BEFORE CONSTRUCTION. THIS INCLUDES, BUT NOT LIMITED TO, TRANSITIONS TO EXISTING CONDITIONS.
- CONSTRUCTION MEANS, METHODS AND JOB SITE SAFETY IS THE SOLE AND EXCLUSIVE RESPONSIBILITY OF THE CONTRACTOR.
- PAVING, SIDEWALK, AND CURBING IS NOT TO BE INSTALLED IN SUCH A WAY THAT IT WILL BLOCK THE FLOW OF WATER AWAY FROM THE BUILDING INCLUDING BUT NOT LIMITED TO WEEP HOLES, WICKS, DRAINAGE SCUPPERS OR PIPES, AND LANDSCAPE.

**GRADING**

- GEOTECHNICAL REPORTS AS PREPARED BY OWNER (OR REPRESENTATIVE) SHALL BE REFERRED TO PRIOR TO EARTH MOVING AND/OR UTILITY CONSTRUCTION.
- UNSTABLE SOIL SHALL BE REMOVED OR STABILIZED.
- CONTRACTOR IS TO MAINTAIN A POSITIVE DRAINAGE PATTERN AT THE END OF EACH DAY. CARE SHOULD BE TAKEN TO INSURE THAT DRAINAGE IS NOT REROUTED OR BLOCKED IN A WAY THAT MAY BE INJURIOUS TO ADJACENT LAND.
- SUBGRADES BELOW STRUCTURES, PAVEMENTS OR NEW STRUCTURAL FILL SHALL BE PROOF ROLLED. IF SOIL RUTS, PUMPS, DEFLECTS EXCESSIVELY OR EXHIBITS EXCESSIVE MOVEMENT OR MOISTURE, THEN THE UNSTABLE SOIL SHALL BE UNDERCUT AND REPLACED WITH STRUCTURAL FILL OR OTHERWISE STABILIZED IN A MANNER THAT MEETS THE COMPACTION REQUIREMENTS. THIS PROCESS IS TO BE OBSERVED BY A GEOTECHNICAL ENGINEER.
- ALL FILLS SHALL BE PLACED IN 6" LIFTS COMPACTED TO A MINIMUM OF 98% STANDARD LABORATORY DENSITY PER ASTM D698 UNDER AND WITHIN INFLUENCE OF THE BUILDING, A MINIMUM OF 95% STANDARD LABORATORY DENSITY PER ASTM D698 UNDER AND WITHIN THE INFLUENCE OF ALL OTHER IMPERVIOUS AREAS, AND A MINIMUM OF 90% STANDARD LABORATORY DENSITY PER ASTM D698 IN ALL LANDSCAPE AREAS.
- EROSION CONTROL SHALL BE PROVIDED PRIOR TO ANY DISTURBANCES. SEE EROSION CONTROL PLANS FOR ADDITIONAL SPECIFICATIONS AND DETAILS.
- PROVIDE TOPSOIL RESPREAD PER THE FOLLOWING UNLESS OTHERWISE NOTED:
  - 4" MINIMUM IN GRASS OR SOD AREAS.
  - 6" MINIMUM IN PLANTING AREAS.
  - 12" MINIMUM IN LANDSCAPE ISLANDS.
- ALL TOPSOIL TO BE FRIABLE (NOT COHESIVE), WEED FREE, AND FREE OF ROCKS, LARGE ROOTS AND UNNATURAL DEBRIS.
- ALL GRADING IS TO BE CONSTRUCTED TO WITHIN A TOLERANCE OF 0.10' OF THE PROPOSED ELEVATIONS. SEE PAVEMENT SPECIFICATIONS FOR PAVEMENT TOLERANCES.

**SANITARY SEWER SPECIFICATIONS**

- ALL SANITARY SEWER PIPE SHALL BE EXTRA STRENGTH V.C.P. CONFORMING TO ASTM C-700 SPECIFICATIONS WITH ASTM C-425 JOINTS; OR D.I.P., MIN. CLASS 50, CONFORMING TO ANSI A-21.51 WITH ANSI A-21.11 JOINTS; OR P.V.C. PIPE CONFORMING TO ASTM D-3034 SPECIFICATIONS, SDR26 WALL THICKNESS AND ASTM D-3212 GASKET TYPE JOINTS OR ASTM D-2855 SOLVENT WELDED JOINTS.
- ALL WATERMAIN QUALITY PLASTIC PIPE SHALL BE P.V.C. CONFORMING TO NSF STANDARD 14 AND; ASTM STANDARD B 1784 OR AWWA STANDARD C900 OR C905. JOINTING SHALL BE PRESSURE SLIP JOINTED. ELASTOMERIC SEALS (GASKETS) USED FOR PUSH-ON JOINTS SHALL COMPLY WITH ASTM STANDARD F477, AND SHALL BE PRESSURE RATED IN ACCORDANCE WITH ASTM D3139.
- DEFLECTION OF POLYVINYL CHLORIDE (PVC) PIPE SHALL NOT EXCEED 5.0% OF THE "BASE I.D." (INTERNAL DIAMETER) OF THE PIPE. "BASE I.D." SHALL BE CALCULATED IN ACCORDANCE WITH THE FOLLOWING:  

$$\text{AVG ID} = \text{AVG OD} - 2(1.06)T$$

$$\text{TOLERANCE PACKAGE} = (A^2 + B^2 + C^2)^{1/2}$$
 WHERE:  
 A = OD TOLERANCE (ASTM D-3034)  
 B = EXCESS WALL THICKNESS TOLERANCE = 0.06T  
 C = OUT-OF-ROUNDNESS TOLERANCE = 0.015 (AVG OD)  
 T = MINIMUM WALL THICKNESS (ASTM D-3034)  
 BASE ID = AVG ID - TOLERANCE PACKAGE  
 DEFLECTION OF COMPOSITE PIPE ("TRUSS" PIPE) SHALL NOT EXCEED 3.0% OF THE AVERAGE INSIDE DIAMETER (ID) OF THE PIPE IN ACCORDANCE WITH ASTM D-2680. THE PIPE LINE SHALL BE TESTED FOR EXCESS DEFLECTING BY PULLING A "GO - NO GO" MANDREL THROUGH THE PIPE FROM MANHOLE TO MANHOLE. THE MANDREL SHALL BE SIZED IN ACCORDANCE WITH SECTION 31-1.11C (4), AND AS SPECIFIED IN THE SPECIAL PROVISIONS. A "DEFLECTOMETER" MAY ALSO BE USED TO CHECK AND RECORD DEFLECTION, WHENEVER POSSIBLE AND PRACTICAL. THE TESTING SHALL INITIATE AT THE DOWNSTREAM LINES AND PROCEED TOWARDS THE UPSTREAM LINES, WHERE THE DEFLECTION IS FOUND TO BE IN EXCESS OF ALLOWABLE TESTING LIMITS, THE CONTRACTOR SHALL EXCAVATE TO THE POINT OF EXCESS DEFLECTION AND CAREFULLY COMPACT AROUND THE POINT WHERE EXCESS DEFLECTION WAS FOUND. THE LINE SHALL THEN BE RETESTED FOR DEFLECTION. HOWEVER, SHOULD AFTER THE INITIAL TESTING THE DEFLECTED PIPE FAIL TO RETURN TO THE ORIGINAL SIZE (INSIDE DIAMETER) THE LINE SHALL BE REPLACED.
- INFILTRATION OR EXFILTRATION SHALL NOT EXCEED 100 GALLONS PER TWENTY-FOUR (24) HOURS PER MILE PER INCH-DIAMETER OF THE SEWER PIPE, FOR ANY SECTION OF THE SYSTEM AND AT ANY TIME DURING ITS SERVICE LIFE. TESTING IS REQUIRED PER THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS."
- LEAKAGE TESTING FOR MANHOLES FOR WATER TIGHTNESS SHALL BE DONE IN ACCORDANCE WITH ASTM C969-94--"STANDARD PRACTICE FOR INFILTRATION AND EXFILTRATION ACCEPTANCE TESTING OF INSTALLED PRECAST CONCRETE PIPE SEWER LINES", VOL. 04.05, CHEMICAL RESISTANT MATERIALS, VITRIFIED CLAY, CONCRETE, FIBER-CEMENT PRODUCTS, MORTARS; MASONRY (1996)(NO LATER EDITIONS OR AMENDMENTS) OR ASTM C1244-93 "STANDARD TEST METHOD FOR CONCRETE SEWER MANHOLES BY THE NEGATIVE PRESSURE (VACUUM) TEST", VOL. 04.05, CHEMICAL RESISTANT MATERIALS, VITRIFIED CLAY, CONCRETE, FIBER-CEMENT PRODUCTS; MORTARS; MASONRY (1996)(NO LATER EDITIONS OR AMENDMENTS) PRIOR TO PLACING INTO SERVICE.
- ALL STRUCTURE LIDS SHALL BE IMPRINTED "SANITARY".
- ALL WATERTIGHT FRAMES AND LIDS SHALL BE NEENAH R-1916-C. ALL OTHER FRAMES AND LIDS SHALL BE NEENAH R-1550-A WITH A CONCEALED PICK HOLE.
- ALL FRAMES AND LIDS SHALL CONFORM TO ASTM A-48.
- ALL SEWERS ARE TO BE INSTALLED FROM THE DOWNSTREAM END UPSTREAM. IF ANY CONFLICTS ARE ENCOUNTERED, THE DESIGN ENGINEER IS TO BE CONTACTED PRIOR TO THE INSTALLATION OF ANY PIPE.
- FOR A DROP CONNECTION, THE DIAMETER OF THE DROP PIPE SHALL PREFERABLY BE LARGER THAN, OR OF THE SAME DIAMETER AS, THE ENTERING SEWER. THE MINIMUM DIAMETER OF THE DROP PIPE SHALL NOT BE SMALLER THAN THE DIAMETER OF THE ENTERING SEWER BY MORE THAN TWO NOMINAL DIAMETERS, PROVIDED THAT THE MINIMUM DIAMETER OF THE DROP PIPE SHALL NOT BE LESS THAN EIGHT INCHES (8").
- ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER.
- ALL SANITARY SEWER BEDDING SHALL BE IN ACCORDANCE WITH THE TRENCH DETAIL AS INCLUDED IN THE PLANS.

**PAVEMENT**

- ALL PAVEMENTS SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE FOLLOWING REFERENCES AS THEY APPLY: STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ILLINOIS DEPARTMENT OF TRANSPORTATION, LATEST EDITION; MANUAL FOR STRUCTURAL DESIGN OF PORTLAND CEMENT CONCRETE PAVEMENT, ILLINOIS DEPARTMENT OF TRANSPORTATION, LATEST EDITION; DESIGN MANUAL, ILLINOIS DEPARTMENT OF TRANSPORTATION, LATEST EDITION.
- ALL PAVED AREAS SHALL BE COMPACTED TO 95% STANDARD LABORATORY DENSITY, PER I.D.O.T. SECTION 301, BEFORE THE BASE COURSE MATERIALS ARE INSTALLED. THE SUB-GRADE SHALL BE PROOF-ROLLED TO THE SATISFACTION OF THE ENGINEER, HIS AGENT, AND/OR THE SOILS ENGINEER. COMPACTION AND DENSITY TESTS SHALL BE TAKEN AT THE OWNER'S OPTION.
- ALL CONCRETE TO BE MINIMUM 3500 PSI, SALT TOLERANT, 6 BAG MIX WITH A SPRAY ON SEALER.
- EXPANSION JOINTS SHALL BE 3/4" WATERPROOF PREFABRICATED FELT BOARD TYPE MATERIAL THE FULL DEPTH OF THE CONCRETE SECTION.
- BINDER COURSE TO BE PLACED WHEN TEMPERATURE IS AT LEAST 40° AND RISING. SURFACE COURSE TO BE PLACED WHEN TEMPERATURE IS AT LEAST 45° AND RISING.
- ALL PROPOSED PAVEMENT AND CURBS ARE TO BE CONSTRUCTED TO WITHIN A TOLERANCE OF 0.05' OF THE PROPOSED ELEVATIONS EXCEPT IN THE ACCESSIBLE STALLS OR ACCESSIBLE ROUTES.
- ALL PROPOSED PAVEMENT, CURBS, SIDEWALKS, RAMPS, ETC. WITHIN THE ACCESSIBLE STALLS AND ACCESSIBLE ROUTES SHALL BE CONSTRUCTED PER THE ILLINOIS ACCESSIBILITY CODE (LATEST EDITION) WITH ZERO CONSTRUCTION TOLERANCE. MAXIMUM SLOPES AS DEFINED IN THE CODE SHALL NOT BE EXCEEDED. THESE MAXIMUM SLOPES INCLUDE, BUT ARE NOT LIMITED TO:
  - PARKING SPACES AND ACCESS AISLES SHALL BE LEVEL WITH SURFACE SLOPES NOT EXCEEDING 1:50 (2.00%) IN ALL DIRECTIONS.
  - AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 1:20 (5.00%) IS A "RAMP" AND SHALL COMPLY WITH THE "RAMP" REQUIREMENTS WHICH INCLUDE, BUT ARE NOT LIMITED TO, HAND RAILS. NOWHERE SHALL THE CROSS SLOPE OF AN ACCESSIBLE ROUTE EXCEED 1:50 (2.00%).
  - THE MAXIMUM SLOPE OF A "RAMP" IN NEW CONSTRUCTION SHALL BE 1:12 (8.33%). THE MAXIMUM RISE FOR ANY RUN SHALL BE 30 INCHES.
- PRIOR TO SEAL COATING, ALL ASPHALT AREAS ARE TO BE CLEAN AND DRY. ALL LOOSE MATERIALS ARE TO BE REMOVED. ALL GREASE TO BE REMOVED. ALL CRACKS ARE TO BE FILLED PER DOT STANDARDS. ALL PAINTED STRIPING TO BE MODIFIED SHALL BE "BLACKED OUT" WITH BLACK PAINT (1 COAT MINIMUM, 2 COATS IF NECESSARY), ALLOWED TO THOROUGHLY DRY PER PAINT MANUFACTURER. PRIOR TO SEAL COATING, ALL AREAS THAT ARE ADJACENT TO THE SEAL COATED AREA ARE TO BE MASKED (I.E. SIDEWALKS, CONCRETE SURFACES, BRICK SURFACES, CUTTERS, CATCHBASINS/INLETS, ETC.) PRIOR TO SEAL COATING TO BE APPLIED. AIR TEMPERATURE TO BE 50° AND RISING. APPLICATION RATE TO BE SUCH THAT ALL SURFACES OF THE ASPHALT BEING COATED IS THOROUGHLY COVERED IN ONE COAT. SPRAYING IS NOT ALLOWED. ALL SEAL COATING SHOULD BE APPLIED BY SQUEEGEE OR BRUSHES. THE BITUMINOUS SEAL COATING MATERIAL SHOULD NOT BE ALLOWED TO ENTER STORM SEWERS AND SHOULD BE ALLOWED TO DRY AT LEAST 18 HOURS PRIOR TO VEHICULAR USE. CRACK FILLER AND SEAL COATING MATERIALS ARE TO BE FREE OF COAL TAR.

**WATER MAIN SPECIFICATIONS**

- HORIZONTAL SEPARATION
  - WATER MAINS AND SEWERS: WATER MAINS SHALL BE LAID AT LEAST TEN FEET HORIZONTALLY FROM ANY EXISTING OR PROPOSED DRAIN, STORM SEWER, SANITARY SEWER, COMBINED SEWER OR SEWER SERVICE CONNECTION.
  - WATER MAINS MAY BE LAID CLOSER THAN TEN FEET TO A SEWER LINE OR SEWER SERVICE CONNECTION WHEN:
    - LOCAL CONDITIONS PREVENT A LATERAL SEPARATION OF TEN FEET;
    - THE WATER MAIN INVERT IS AT LEAST EIGHTEEN INCHES (18") ABOVE THE CROWN OF THE SEWER; AND
    - THE WATER MAIN IS EITHER IN A SEPARATE TRENCH OR IN THE SAME TRENCH ON AN UNDISTURBED EARTH SHELF LOCATED TO ONE SIDE OF THE SEWER.
  - BOTH THE WATER MAIN AND SEWER PIPE SHALL BE CONSTRUCTED OF PUSH JOINT OR MECHANICAL JOINT DUCTILE IRON PIPE, PRESSURE PIPE, PRESTRESSED CONCRETE PIPE, OR PVC SDR18 PIPE WITH AWWA C-900 JOINTS, MEETING THE REQUIREMENTS OF SECTION 653.111 OF THE IEPA'S TITLE 35 SUBTITLE F, WHEN IT IS IMPOSSIBLE TO MEET (A) OR (B) ABOVE. THE DRAIN OR SEWER SHALL BE PRESSURE TESTED TO THE MAXIMUM EXPECTED SURCHARGE HEAD BEFORE BACKFILLING.
- VERTICAL SEPARATION
  - A WATER MAIN SHALL BE LAID SO THAT ITS INVERT IS EIGHTEEN INCHES (18") ABOVE THE CROWN OF THE DRAIN OR SEWER WHENEVER WATER MAINS CROSS STORM SEWERS, SANITARY SEWERS OR SEWER SERVICE CONNECTIONS. THE VERTICAL SEPARATION SHALL BE MAINTAINED FOR THAT PORTION OF THE WATER MAIN LOCATED WITHIN TEN FEET HORIZONTALLY OF ANY SEWER OR DRAIN CROSSING. A LENGTH OF WATER MAIN PIPE SHALL BE CENTERED OVER THE SEWER TO BE CROSSED WITH JOINTS EQUIDISTANT FROM THE SEWER OR DRAIN.
  - BOTH THE WATER MAIN AND SEWER SHALL BE CONSTRUCTED OF PUSH JOINT OR MECHANICAL JOINT DUCTILE PIPE, PRESTRESSED CONCRETE PIPE, OR PVC PIPE MEETING THE REQUIREMENTS OF SECTION 635.111 OF THE IEPA'S TITLE F, SUBTITLE F, AND CONSTRUCTION SHALL EXTEND ON EACH SIDE OF THE CROSSING UNTIL THE PERPENDICULAR DISTANCE FROM THE WATER MAIN TO THE SEWER DRAIN LINE IS AT LEAST TEN FEET (10') WHEN:
    - IT IS IMPOSSIBLE TO OBTAIN THE PROPER VERTICAL SEPARATION AS DESCRIBED IN (A) ABOVE; OR
    - THE WATER MAIN PASSES UNDER A SEWER OR DRAIN.
  - A VERTICAL SEPARATION OF EIGHTEEN INCHES (18") BETWEEN THE INVERT OF THE SEWER OR DRAIN AND THE CROWN OF THE WATER MAIN SHALL BE MAINTAINED WHERE A WATER MAIN CROSSES UNDER A SEWER. SUPPORT THE SEWER OR DRAIN LINES TO PREVENT SETTLING AND BREAKING THE WATER MAIN.
- WATER MAINS AND SERVICES SHALL BE CONSTRUCTED SO THAT THE MINIMUM DEPTH IS FIVE AND ONE HALF FEET (5 1/2') MEASURED FROM FINISHED GRADE TO THE TOP OF THE PIPE, UNLESS OTHERWISE SPECIFIED AND/OR APPROVED BY THE REVIEW ENGINEER.
- ALL WATER MAIN FITTINGS MAY OR MAY NOT BE SHOWN ON THE PLANS AND SHOULD BE INCLUDED IN THE COST OF THE WATER MAIN ITSELF FOR BIDDING PURPOSES. ALL WATER MAIN SHALL BE DUCTILE IRON CLASS 52 CEMENT LINED CONFORMING TO ANSI A-21.51 WITH ANSI A-21.11 JOINTS, OR TYPE "K" COPPER PIPE WITH SWEATED JOINTS.
- FIRE HYDRANTS SHALL MEET AWWA C-502 AND BE TRAVERSE CITY IRON WORKS, EAST JORDAN 5 BR, OR APPROVED EQUAL, WITH FIVE AND ONE QUARTER INCH (5 1/4") VALVE OPENING, TWO TWO AND ONE HALF INCH (2 1/2") HOSE NOZZLES AND ONE FIVE INCH (5") PUMPER NOZZLE. FIRE HYDRANT SHALL BE EQUIPPED WITH AN AUXILIARY RESILIENT SEAL GATE VALVE COMPLETE WITH ROADWAY BOX, TYLER, 6850 SERIES, ITEM 66B-S. FIRE HYDRANTS MUST HAVE THEIR DISCHARGE AT LEAST 18 INCHES BUT NOT MORE THAN TWENTY-FOUR INCHES (24") FROM THE SURFACE OF THE ADJACENT GROUND.
- HYDRANTS SHALL BE INSTALLED NO CLOSER THAN THREE FEET (3') NOR FURTHER THAN EIGHT FEET (8') FROM THE BACK OF CURB OR EDGE OF PAVEMENT TO THE FIVE INCH (5") STEAMER NUT, NO BARRIERS, TREES, BUSHES, WALLS OR OTHER OBSTACLES WHICH MAY HIDE OR IMPEDE THE USE OF A FIRE HYDRANT SHALL BE INSTALLED, MAINTAINED, CONSTRUCTED, OR ENLARGED, WITHIN FORTY-EIGHT INCHES (48") OF A HYDRANT.
- ALL STRUCTURE LIDS SHALL BE IMPRINTED "WATER".
- ALL WATERTIGHT FRAMES AND LIDS SHALL BE NEENAH R-1916-C. ALL OTHER FRAMES AND LIDS SHALL BE NEENAH R-1550-A WITH A CONCEALED PICK HOLE.
- ALL FRAMES AND LIDS SHALL CONFORM TO ASTM A-48.
- BEFORE BEING PLACED INTO SERVICE, ALL NEW MAINS AND REPAIRED PORTIONS OF, OR EXTENSIONS TO EXISTING MAINS SHALL BE CHLORINATED SO THAT THE INITIAL CHLORINE RESIDUAL IS NOT LESS THAN FIFTY (50) MG/L AND THAT A CHLORINE RESIDUAL OF NOT LESS THAN TWENTY-FIVE (25) MG/L REMAINS IN THE WATER AFTER STANDING TWENTY-FOUR (24) HOURS IN THE PIPE.

DATE	07/08/14
REVISED	07/28/14
NO.	1
NO.	2

Prepared For:

Lingie Design Group  
158 W. Main Street  
Lena, IL 61048

McDonald's - VERNON HILLS, IL  
700 N. Milwaukee Avenue  
Vernon Hills, Illinois

Prepared By:

Watermark Engineering Resources, Ltd.  
2031 Ginger Woods Parkway, Suite 100, Aurora, IL 60202  
phone 630-375-1800 fax 630-258-0800 www.watermark-engineering.com

CHECKED BY:	J. MILLER
DESIGN BY:	M. BAILEY
DRAWN BY:	K. SACK
DATE:	NOVEMBER 21, 2013
SCALE:	NONE
PROJECT NO.:	13-010

**PROJECT SPECIFICATIONS**

**C-6**

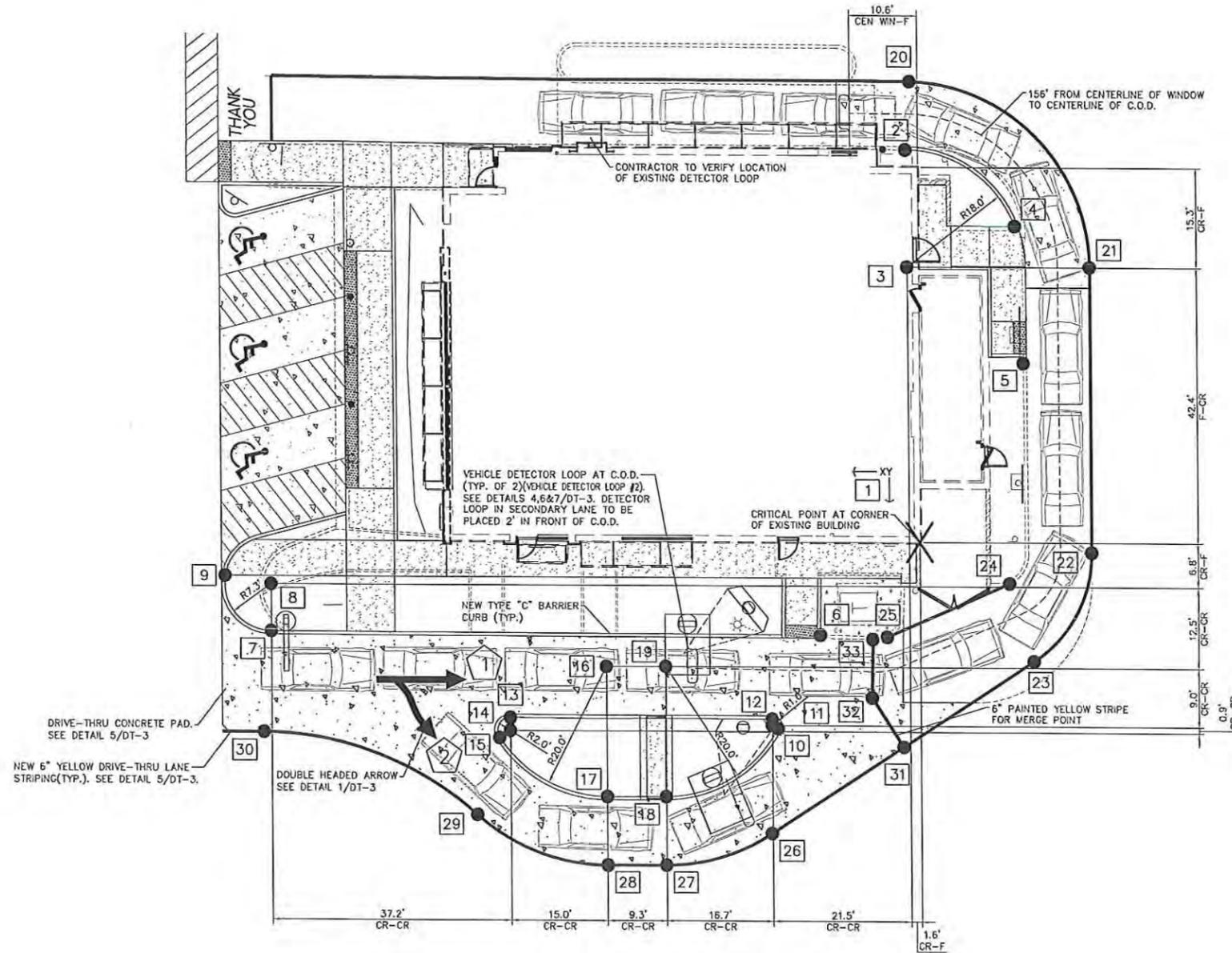
LC #12-1085

2\cgs\13\1085\13010\13010.dwg, 13/10

PROJECT SPECIFICATIONS

**GENERAL NOTES:**

1. THE REGIONAL CONSTRUCTION MANAGER IS TO REVIEW AND APPROVE ALL DRIVE-THRU LAYOUTS. A DRIVE-THRU IS FINAL, AND CONSIDERED "RED", ONCE APPROVED. NO CHANGES ARE TO BE MADE AFTER THIS POINT.
2. DUE TO THE EXACT GEOMETRY REQUIRED FOR THE EFFICIENT OPERATION OF THIS DRIVE-THRU LAYOUT, IT IS RECOMMENDED THAT ALL DRIVE-THRU EQUIPMENT AND PAVEMENT IMPROVEMENTS TO BE FIELD LOCATED BY A LICENSED SURVEYOR.
3. THE PLACEMENT OF THE CODs AND ANY ADDITIONAL SIGNAGE SHOULD BE SUCH THAT IT PREVENTS, OR MINIMIZES, BLOCKING THE CUSTOMER'S VIEW OF THE MENU BOARD WHILE ORDERING.
4. THESE DIMENSIONS ARE CRITICAL TO THE FUNDAMENTAL LAYOUT OF THE SIDE BY SIDE DESIGN.
5. IF DIMENSIONS ARE MODIFIED CONTACT DESIGN ENGINEER IMMEDIATELY.
6. VERIFY WITH SUPPLIER OF DRIVE-THRU EQUIPMENT THAT MOST CURRENT EQUIPMENT IS BEING UTILIZED.



COORDINATES			
Pavement	X	Y	DESCRIPTION
1	0.00	0.00	CRITICAL STARTING POINT FOR ALL COORDINATES
2	1.77	-80.42	TO BACK OF CURB AT START OF RADIUS
3	1.63	-42.42	TO CENTER OF RADIUS(15)
4	-15.22	-48.73	TO BACK OF CURB AT END OF RADIUS
5	-16.42	-27.66	TO BACK OF CURB
6	15.56	14.33	TO BACK OF CURB
7	101.27	14.10	TO BACK OF CURB AT START OF RADIUS
8	101.27	8.84	TO CENTER OF RADIUS(7.3)
9	108.42	5.60	TO BACK OF CURB AT END OF RADIUS
10	22.21	28.61	TO BACK OF CURB AT START/END OF RADIUS
11	23.09	28.33	TO CENTER OF RADIUS(1)
12	23.08	27.33	TO BACK OF CURB AT END OF RADIUS
13	54.01	27.25	TO BACK OF CURB AT START OF RADIUS
14	84.09	28.25	TO CENTER OF RADIUS(2)
15	86.78	30.35	TO BACK OF CURB AT END START OF RADIUS
16	49.09	19.33	TO CENTER OF RADIUS(20)
17	49.02	35.33	TO BACK OF CURB AT END OF RADIUS
18	39.83	35.31	TO BACK OF CURB AT START OF RADIUS
19	39.81	19.31	TO CENTER OF RADIUS(20)
20	1.04	-70.90	TO STRIPING CONCRETE
21	-28.87	-42.47	TO STRIPING CONCRETE
22	-28.85	1.49	TO STRIPING CONCRETE
23	-17.77	18.29	TO STRIPING CONCRETE
24	-14.00	6.31	TO STRIPING CONCRETE
25	5.17	14.65	TO STRIPING CONCRETE
26	23.20	44.67	TO STRIPING CONCRETE
27	39.84	49.61	TO STRIPING CONCRETE
28	48.92	49.63	TO STRIPING CONCRETE
29	89.29	42.17	TO STRIPING CONCRETE
30	102.45	28.60	TO STRIPING CONCRETE
31	2.68	31.55	TO MERGE POINT STRIPING
32	7.59	24.01	TO MERGE POINT STRIPING
33	7.42	15.01	TO MERGE POINT STRIPING

DATE	REVISIONS
01/09/14	NO REVISIONS
01/28/14	REV X-BASE REVISION

Prepared For:

Lingle Design Group  
158 W. Main Street  
Lena, IL 61048

McDonald's - VERNON HILLS, IL  
700 N. Milwaukee Avenue  
Vernon Hills, Illinois

Prepared By:

**Watermark Engineering Resources, LTD**  
2631 Chestnut Parkway, Suite 100  
Phone 630-375-1800 Fax 630-375-1800 www.watermark-engineering.com

CHECKED BY: J. MILLER  
DESIGN BY: M. BAILEY  
DRAWN BY: K. SACK  
DATE: NOVEMBER 21, 2013  
SCALE: 1" = 10'  
PROJECT NO.: 13-010

**1 DRIVE-THRU APPROACH**

DT-1 1" = 10'-0"

1 PRIMARY DRIVE-THRU LANE

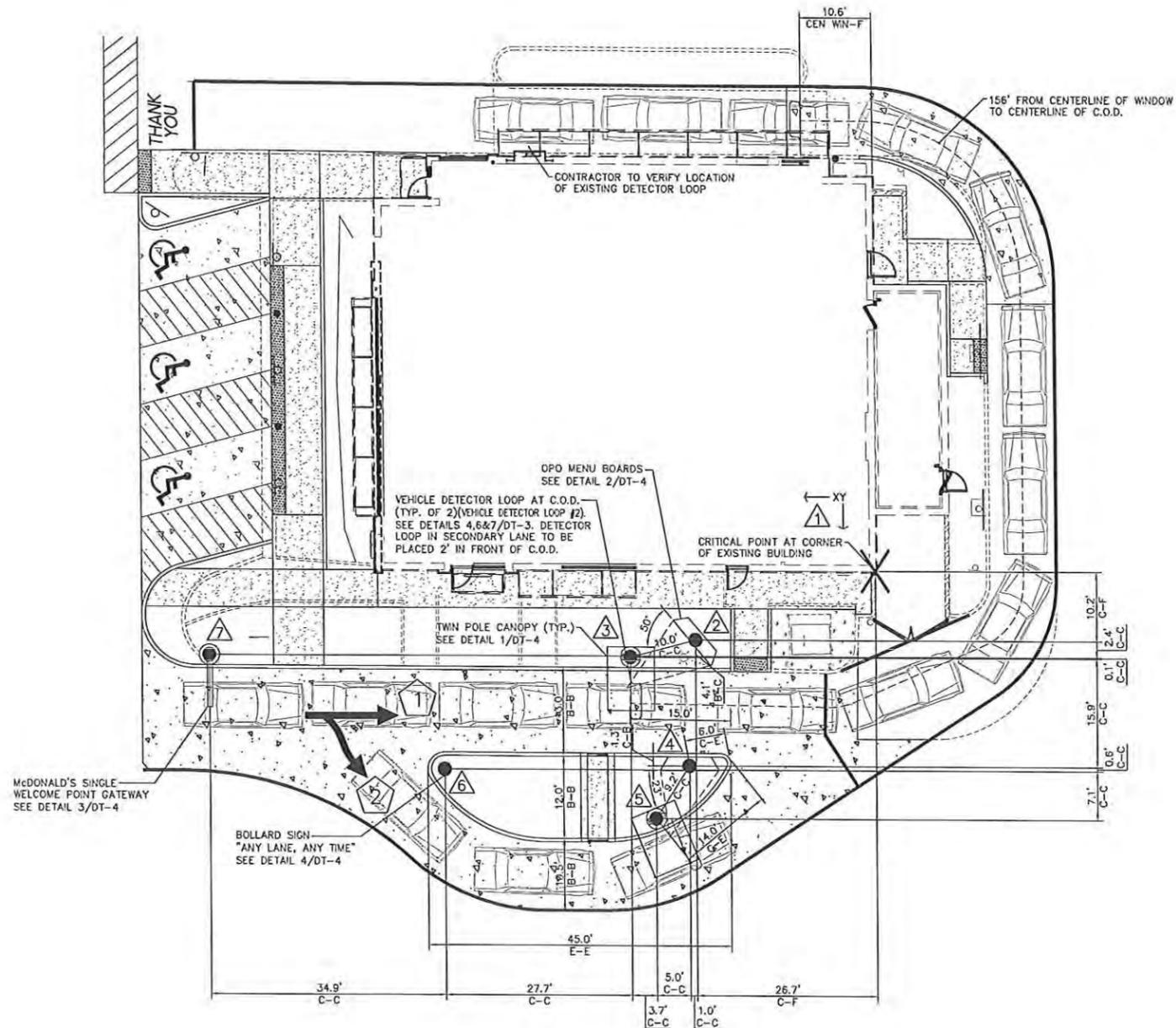
2 SECONDARY DRIVE-THRU LANE



**DRIVE-THRU PAVEMENT PLAN**

DT-1  
LC #12-1085

DRIVE-THRU PAVEMENT PLAN



- GENERAL NOTES:**
1. THE LOCATION AND ORIENTATION OF MENU BOARDS WERE DETERMINED BY THE SIGHT LINES OF THE CARS. THE CARS WERE POSITIONED ACCORDING TO THE MAX. AMOUNT OF CARS DURING A PEAK PERIOD. ALL DIMENSIONS SHOWN ARE BASED UPON PROTOTYPICAL LAYOUTS SHOWN. ACTUAL DIMENSIONS TO BE BASED ON SITE SPECIFIC CONFIGURATIONS AND THE REQUIREMENTS OF NOTE 2.
  2. MENU BOARD IS ORIENTATED FOR VIEWING BY CAR 9 (PRIMARY USER) AND CAR 8 (SECONDARY USER).
  3. PROVIDE TWO CIRCUITS FOR MENU BOARD AND PRE-SELL BOARDS. ONE CIRCUIT FOR LIGHTS AND ONE CIRCUIT WITH LOCK FOR MOTOR AND CONTROLLER. REFER TO ARCHITECTURAL/ELECTRICAL PLANS FOR TERMINATION OF CONDUIT RUNS INSIDE BUILDING.
  4. ALL GALVANIZED ANCHOR BOLTS TO BE SUPPLIED AND INSTALLED BY THE CONTRACTOR.
  5. **PLACEMENT NOTE:** THE PLACEMENT OF THE CUSTOMER ORDER DISPLAY SHOULD BE SUCH THAT IT DOES NOT BLOCK THE VIEWING OF THE MENU BOARD FROM THE DRIVER'S VANTAGE POINT.
  6. **VERIFICATION NOTE:** VERIFY ALL DRIVE-THRU EQUIPMENT WITH McDONALD'S PROJECT MANAGER AND OPERATIONS MANAGER BEFORE PROCEEDING.
  7. THE REGIONAL CONSTRUCTION MANAGER IS TO REVIEW AND APPROVE ALL DRIVE-THRU LAYOUTS. A DRIVE-THRU IS FINAL, AND CONSIDERED "RED", ONCE APPROVED. NO CHANGES ARE TO BE MADE AFTER THIS POINT.
  8. DUE TO THE EXACT GEOMETRY REQUIRED FOR THE EFFICIENT OPERATION OF THIS DRIVE-THRU LAYOUT, IT IS RECOMMENDED THAT ALL DRIVE-THRU EQUIPMENT AND PAVEMENT IMPROVEMENTS TO BE FIELD LOCATED BY A LICENSED SURVEYOR.
  9. THE PLACEMENT OF THE C.O.D.s AND ANY ADDITIONAL SIGNAGE SHOULD BE SUCH THAT IT PREVENTS, OR MINIMIZES, BLOCKING THE CUSTOMER'S VIEW OF THE MENU BOARD WHILE ORDERING.
  10. THESE DIMENSIONS ARE CRITICAL TO THE FUNDAMENTAL LAYOUT OF THE SIDE BY SIDE DESIGN.
  11. IF DIMENSIONS ARE MODIFIED CONTACT DESIGN ENGINEER IMMEDIATELY.
  12. VERIFY WITH SUPPLIER OF DRIVE-THRU EQUIPMENT THAT MOST CURRENT EQUIPMENT IS BEING UTILIZED.

COORDINATES			
Equipment	X	Y	DESCRIPTION
1	0.00	0.00	CRITICAL STARTING POINT FOR ALL COORDINATES
2	26.71	10.23	TO CENTER OF FOUNDATION OF OPO MENU BOARD
3	36.38	12.74	TO CENTER OF FOUNDATION OF PRIMARY LANE DRIVE-THRU TWIN POLE CANOPY
4	27.70	28.86	TO CENTER OF FOUNDATION OF OPO MENU BOARD
5	32.67	36.53	TO CENTER OF FOUNDATION OF SECONDARY LANE DRIVE-THRU TWIN POLE CANOPY
6	64.09	29.25	TO CENTER OF BOLLARD SIGN
7	99.03	12.61	TO CENTER OF FOUNDATION OF SINGLE WELCOME POINT GATEWAY SIGN

NO.	DATE	REVISIONS
1	03/08/14	NO REVISIONS
2	07/28/14	REV. X-BASE REVISION

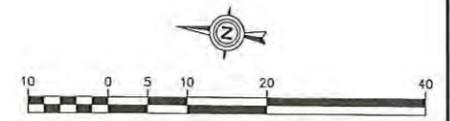
Prepared For:  
 Lingie Design Group  
 158 W. Main Street  
 Lena, IL 61048  
 McDONALD'S - VERNON HILLS, IL  
 700 N. Milwaukee Avenue  
 Vernon Hills, Illinois

Prepared By:  
 Watsonmark Engineering Resources, Ltd.  
 2631 Chester Woods Parkway, Suite 100, Aurora, IL 60202  
 phone 630-371-1800 fax 630-236-9100 www.watsonmark-engineering.com

**1 DRIVE-THRU APPROACH**

DT-2 1" = 10'-0"

- 1 PRIMARY DRIVE-THRU LANE
- 2 SECONDARY DRIVE-THRU LANE



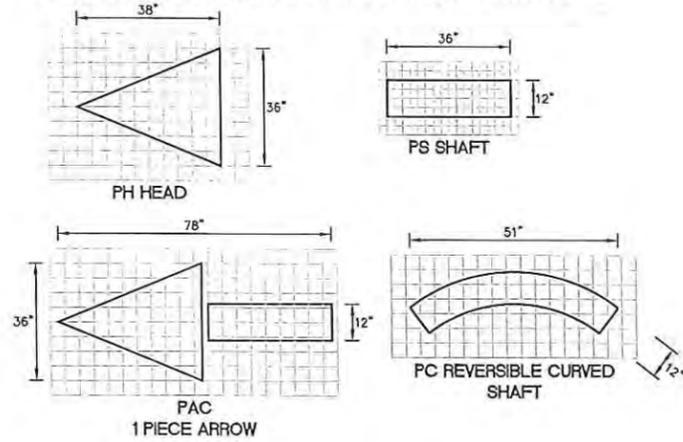
**DRIVE-THRU EQUIPMENT PLAN**

CHECKED BY: J. MILLER	PROJECT NO: 13-010
DESIGN BY: M. BAILEY	SCALE: 1" = 10'
DRAWN BY: K. SACK	DATE: NOVEMBER 21, 2013
DATE: NOVEMBER 21, 2013	
LC #12-1085	

DRIVE-THRU EQUIPMENT PLAN

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**PARKING LOT ARROWS** (MEETS NO STATE OR FEDERAL DESIGN STANDARDS)

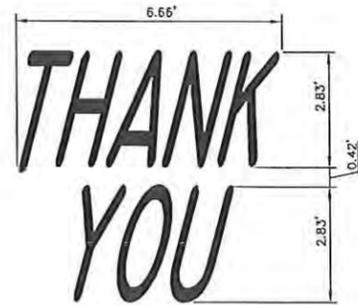


1 TYPICAL PAVEMENT MARKING DETAIL  
DT-3



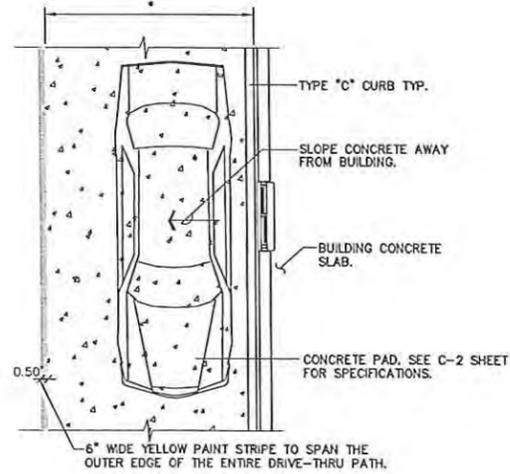
NOTE: ALL TEXT AND ARROW SHALL BE PAINTED YELLOW.

2 PAINTED 'DRIVE-THRU' WITH ARROW DETAIL  
DT-3



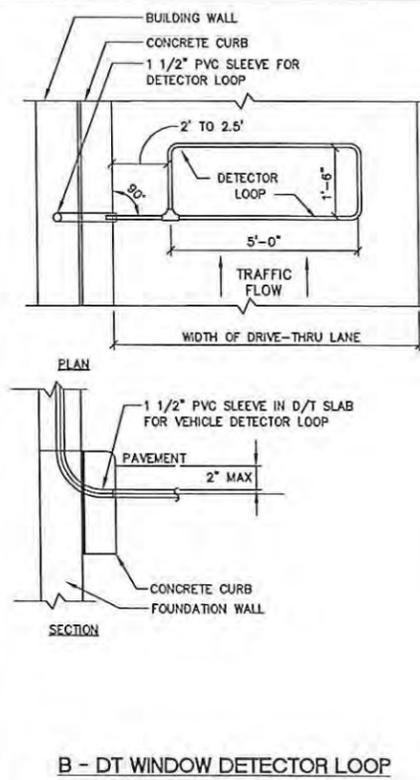
NOTE: ALL TEXT SHALL BE PAINTED YELLOW.

3 PAINTED 'THANK-YOU' DETAIL  
DT-3

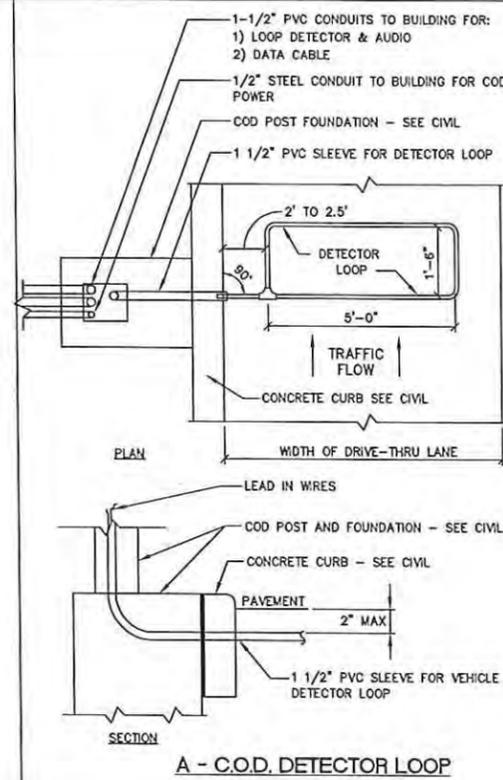


\* FOR DIMENSIONS OF DRIVE-THRU LANE CONCRETE PAD, SEE C-2 SHEET FOR SPECIFICATIONS.

5 DRIVE-THRU CONCRETE PAD DETAIL  
DT-3



B - DT WINDOW DETECTOR LOOP

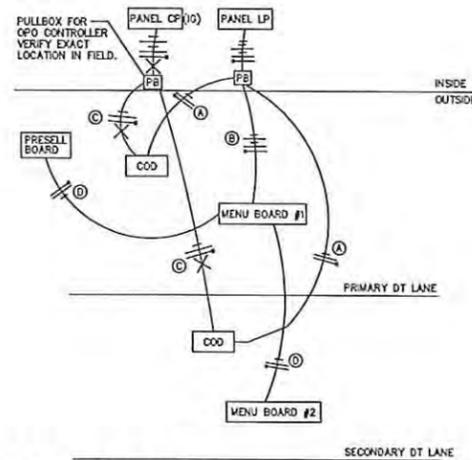


A - C.O.D. DETECTOR LOOP

**NOTES**

1. VERIFY CONDUIT SIZES AND LAYOUT WITH DETECTOR LOOP MANUFACTURER.
2. CENTER VEHICLE DETECTOR LOOP (ITEM # 217.11E1) IN DRIVE THRU LANE. INSTALL PER MFR. RECOMMENDATIONS.
3. SEE CIVIL FOR DIMENSIONS OF DRIVE-THRU LANE CONCRETE PAD FOR DETECTOR LOOP.
4. NO STEEL (REBAR OR ELECTRICAL WIRE) SHALL BE USED WITHIN 2' OF LOOP.
5. DETECTOR LOOP MANUFACTURERS: DETECTOR LOOPS MAY BE BY ONE OF THE FOLLOWINGS COMPANIES OR EQUAL.  
SM: 1-800-328-0033  
HWE: 1-800-848-4468
6. DETECTOR LOOP MATERIAL: PVC TUBING 1/2" I.D., 100 PSI LOOP MADE FROM ONE LENGTH OF THIN FOURTEEN GAUGE STRANDED WIRE. LEAD-IN IS PRE-TWISTED AT FACTORY.
7. DETECTOR LOOP CONSTRUCTION: FORMED WITH ONE CONTINUOUS LENGTH OF PVC WITH NO SHARP CORNERS AS DETAILED. WIRE LOOPED, FORMED, & FIGTAILED AS DETAILED.

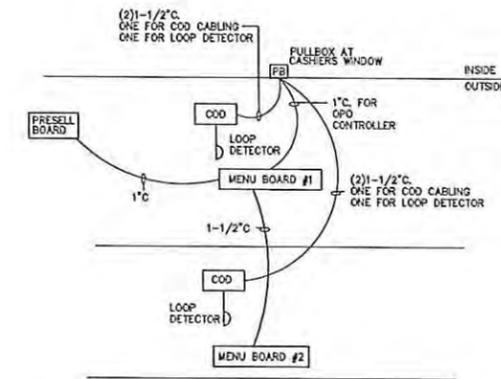
4 DETECTOR LOOP DETAILS  
DT-3 NOT TO SCALE



- (A) 2#12 & 1#12 GND., TO LP-1 FOR COO CANOPY LIGHTING.
- (B) 1#12 & 1#12 GND., 3/4" TO LP-1 FOR OPO MENU BOARD #1 LIGHTING AND PLC.
- (C) 2#12 & 1#12 GND & 1#12 ISOLATED GND., TO CP FOR ISOLATED GROUND POWER TO COO'S. EACH COO SHALL BE ON ITS OWN SEPARATE CIRCUIT.
- (D) 2#12 & 1#12 GND., TO LP-1 FOR OPO MENU BOARD #2 AND PRESSELL BOARD LIGHTING.

- \* NOTE-VERIFY EXACT CIRCUITS & QUANTITIES OF CIRCUITS WITH PANEL SCHEDULES ON DRAWING E4.2 AND MANUFACTURERS INSTALLATION INSTRUCTIONS.
- \* NOTE-PER MANUFACTURER'S INSTALLATION INSTRUCTIONS SEPARATE DEDICATED NEUTRALS ARE REQUIRED TO MENU BOARD AND PRESSELL BOARD FOR EACH CIRCUIT (PLC AND LIGHTING).
- \* CONTRACTOR TO FOLLOW ALL LATEST ELECTRICAL AND BUILDING CODES PER MUNICIPALITIES.

6 DRIVE-THRU POWER DIAGRAM  
DT-3



7 DRIVE-THRU LOW VOLTAGE CONDUIT DIAGRAM  
DT-3

DATE	01.09.14
REVISED	01.28.14
NO. REVISIONS	NO REVISIONS
DATE	01.28.14
REVISED	NO REVISIONS
NO. REVISIONS	NO REVISIONS

Prepared For:

Lingle Design Group  
158 W. Main Street  
Lena, IL 61048  
**McDonald's - VERNON HILLS, IL**  
700 N. Milwaukee Avenue  
Vernon Hills, Illinois

Prepared By:

**Watermark Engineering**  
RESOURCES, LTD.  
2031 Clinger Woods Parkway, Suite 100, Aurora, IL 60502  
phone 630-375-1660 fax 630-258-9880 www.watermark-engineering.com

CHECKED BY:	J. MILLER
DESIGN BY:	M. BAILEY
DRAWN BY:	K. SACK
DATE:	NOVEMBER 21, 2013
SCALE:	NONE
PROJECT NO.:	13-010

**DRIVE-THRU DETAILS**

DT-3  
LC #12-1085

DRIVE-THRU DETAILS



**STRUCTURAL NOTES:**

**DESIGN CODES:**

- IBC 2009
- ASCE 7-05
- AASHTO "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINARIES AND TRAFFIC SIGNALS," FIFTH EDITION, 2009
- ACI 318-08
- AISC 13TH EDITION
- AWS D1.1

**MATERIAL SPECIFICATIONS:**

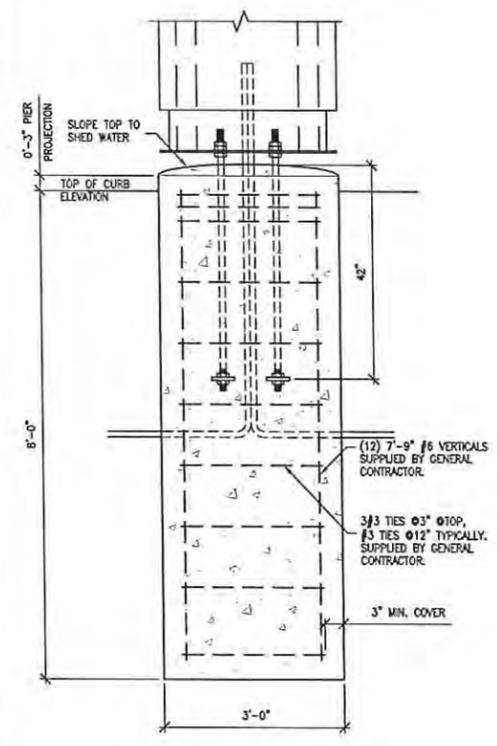
1. CONCRETE COMPRESSIVE STRENGTH (f'c) SHALL BE A MINIMUM OF 3000psi
2. ANCHOR BOLTS SHALL BE ASTM F1554 GRADE 36 OR GRADE 55, AS NOTED, HOT DIP GALVANIZED PER ASTM F2329
3. REINFORCING STEEL SHALL BE ASTM A615 GRADE 60, SUPPLIED BY GENERAL CONTRACTOR
4. NUTS SHALL BE HEAVY HEX ASTM A563, HOT DIP GALVANIZED PER ASTM A153
5. PLATE SHALL BE ASTM A36, HOT DIP GALVANIZED PER ASTM A153
6. LOCK NUT SHALL BE HOT DIP GALVANIZED PER ASTM A153

**FOUNDATION DESIGN PARAMETERS:**

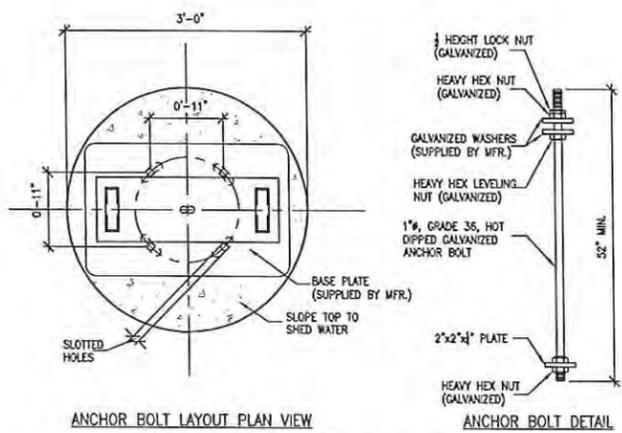
1. MAXIMUM WIND SPEED (3 SECOND GUST) = 90MPH
2. MINIMUM REQUIRED SOIL PARAMETERS:
  - COHESIVE SOILS:
    - SHEAR STRENGTH = 750 lbs/ft<sup>2</sup>
    - 6" MAXIMUM DEPTH OF DISTURBED SOIL OR TOP SOIL
  - COHESIONLESS SOILS:
    - ANGLE OF INTERNAL FRICTION = 27 DEGREES
    - WATER TABLE SHALL BE LOCATED BELOW THE BOTTOM OF THE FOUNDATION
    - 6" MAXIMUM DEPTH OF DISTURBED SOIL OR TOP SOIL
  - MINIMUM ALLOWABLE SOIL BEARING CAPACITY = 3,000 lbs/ft<sup>2</sup>
3. PRIOR TO FABRICATION OF MATERIALS TO BE USED AT THIS SITE, THE GENERAL CONTRACTOR SHALL EMPLOY A SOILS TESTING AGENCY TO PREPARE A SITE SPECIFIC SOIL REPORT TO ESTABLISH THE SOIL PARAMETERS NOTED ABOVE. IF THE MINIMUM PARAMETERS ARE NOT MET, THIS DESIGN SHALL NOT BE USED.
4. ALL EXCAVATIONS SHALL BE INSPECTED BY A SOILS TESTING AGENCY TO VERIFY THE SITE SOIL CONDITIONS MEET OR EXCEEDED THE PARAMETERS LISTED ABOVE BEFORE THIS DESIGN IS USED.
5. THE ENGINEER OF RECORD SHALL REVIEW THE MAXIMUM BASE REACTIONS AND DESIGN WIND SPEED FOR THE UNIT TO BE INSTALLED TO DETERMINE IF THE FOUNDATION'S MAXIMUM DESIGN LOADS HAVE NOT BEEN EXCEEDED. THIS FOUNDATION DESIGN SHALL NOT BE USED IF THE MAXIMUM DESIGN LOADS OR WIND SPEED HAVE BEEN EXCEEDED.
6. THIS FOUNDATION DESIGN SHALL NOT BE USED IN LOCATIONS WHICH ARE CLOSER THAN 8ft FROM A RETAINING WALL.
7. THIS FOUNDATION DESIGN SHALL NOT BE USED AT LOCATIONS WHERE THE GROUND SLOPE EXCEEDS 4 INCHES PER FOOT.

**GENERAL NOTES:**

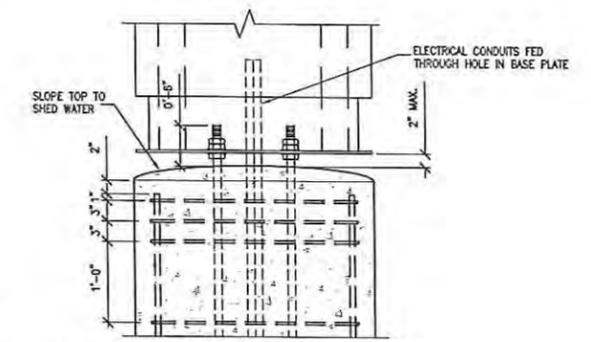
1. PIER DEPTHS SHOWN ARE REQUIRED MINIMUMS. ALL PIERS TO EXTEND TO FROST DEPTH AS DETERMINED BY LOCAL JURISDICTION.
2. TOP OF PIERS SHALL BE SLOPED SUCH THAT MOISTURE CANNOT ACCUMULATE.
3. TOP 6" OF SOIL NEGLECTED IN EMBEDMENT DEPTH CALCULATIONS, (EMBEDMENT DEPTHS SHOWN ARE FORM GRADE).
4. PROVIDE 3" MINIMUM CONCRETE COVER TO REINFORCING BARS, UNLESS NOTED OTHERWISE.
5. ALL REINFORCING BY GENERAL CONTRACTOR.
6. ELECTRICAL CONTRACTOR TO PROVIDE INFORMATION ON CONDUIT AND ELECTRICAL REQUIREMENTS.



**(A) FOUNDATION SECTION**  
3/4" = 1'-0"

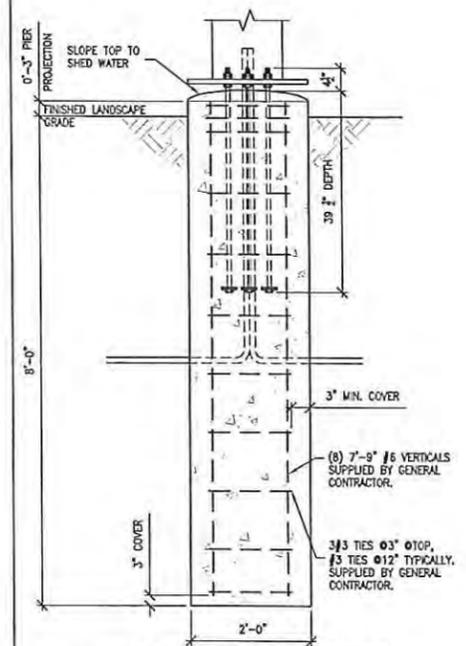


**(B) ANCHOR BOLT DETAILS**  
1" = 1'-0"

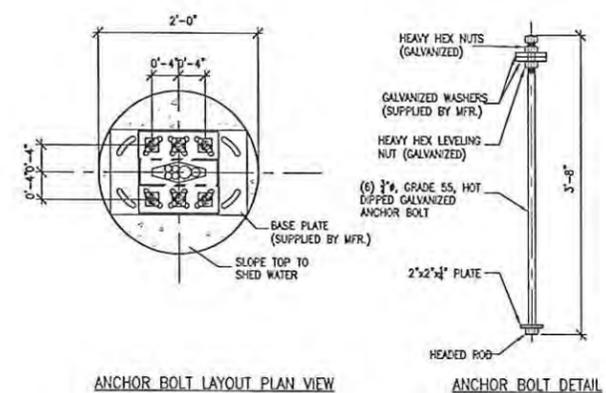


**(C) CONNECTION DETAIL**  
1" = 1'-0"

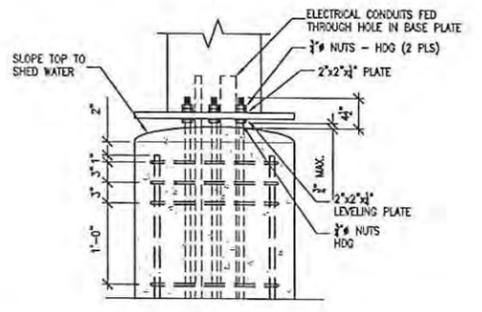
**DRIVE-THRU TWIN POLE CANOPY WITH BUILT-IN COD FOUNDATION**



**(A) FOUNDATION SECTION**  
3/4" = 1'-0"



**(B) ANCHOR BOLT DETAILS**  
1" = 1'-0"



**(C) CONNECTION DETAIL**  
1" = 1'-0"

**OPO OUTDOOR MENU BOARD FOUNDATION**

DATE	
REVIEWS	
NO.	
Prepared For:	
Prepared By:	
CHECKED BY: JAS	
DESIGN BY: SN	
DRAWN BY: GM	
DATE: MAY 30, 2013	
SCALE: NOTED	
PROJECT NO.: 13-010	
<b>SSD-1</b>	
LC #12-1085	

Lingle Design Group, Inc.  
158 West Main Street  
Lena, IL 61048

**McDonald's - VERNON HILLS, IL**  
700 North Milwaukee Avenue  
Vernon Hills, Illinois

SAMARTANO AND COMPANY  
STRUCTURAL ENGINEERS  
221 NORTH LASALLE STREET  
CHICAGO ILLINOIS 60601  
312.322.2326



DRIVE-THRU CANOPY + MENU BOARD FOUNDATIONS

**STRUCTURAL NOTES:**

**DESIGN CODES:**

- IBC 2009
- ASCE 7-05
- AASHTO "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINARIES AND TRAFFIC SIGNALS," FIFTH EDITION, 2009
- ACI 318-08
- AISC 13TH EDITION
- AWS D1.1

**MATERIAL SPECIFICATIONS:**

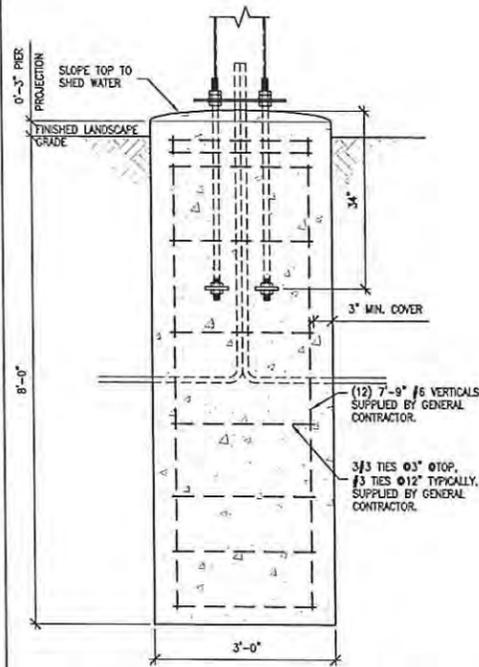
1. CONCRETE COMPRESSIVE STRENGTH ( $f'_c$ ) SHALL BE A MINIMUM OF 3000psi
2. ANCHOR BOLTS SHALL BE ASTM F1554 GRADE 36 OR GRADE 55, AS NOTED, HOT DIP GALVANIZED PER ASTM F2329
3. REINFORCING STEEL SHALL BE ASTM A615 GRADE 60, SUPPLIED BY GENERAL CONTRACTOR
4. NUTS SHALL BE HEAVY HEX ASTM A563, HOT DIP GALVANIZED PER ASTM A153
5. PLATE SHALL BE ASTM A36, HOT DIP GALVANIZED PER ASTM A153
6. LOCK NUT SHALL BE HOT DIP GALVANIZED PER ASTM A153

**FOUNDATION DESIGN PARAMETERS:**

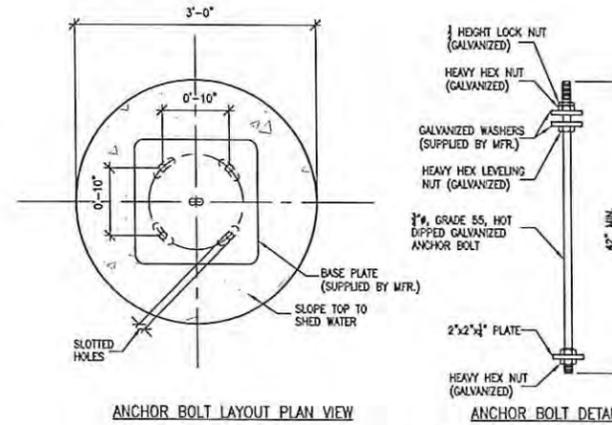
1. MAXIMUM WIND SPEED (3 SECOND GUST) = 90MPH
2. MINIMUM REQUIRED SOIL PARAMETERS:
  - COHESIVE SOILS:
    - SHEAR STRENGTH = 750 lbs/ft<sup>2</sup>
    - 6" MAXIMUM DEPTH OF DISTURBED SOIL OR TOP SOIL
  - COHESIONLESS SOILS:
    - ANGLE OF INTERNAL FRICTION = 27 DEGREES
    - WATER TABLE SHALL BE LOCATED BELOW THE BOTTOM OF THE FOUNDATION
    - 6" MAXIMUM DEPTH OF DISTURBED SOIL OR TOP SOIL
  - MINIMUM ALLOWABLE SOIL BEARING CAPACITY = 3,000 lbs/ft<sup>2</sup>
3. PRIOR TO FABRICATION OF MATERIALS TO BE USED AT THIS SITE, THE GENERAL CONTRACTOR SHALL EMPLOY A SOILS TESTING AGENCY TO PREPARE A SITE SPECIFIC SOIL REPORT TO ESTABLISH THE SOIL PARAMETERS NOTED ABOVE. IF THE MINIMUM PARAMETERS ARE NOT MET, THIS DESIGN SHALL NOT BE USED.
4. ALL EXCAVATIONS SHALL BE INSPECTED BY A SOILS TESTING AGENCY TO VERIFY THE SITE SOIL CONDITIONS MEET OR EXCEEDED THE PARAMETERS LISTED ABOVE BEFORE THIS DESIGN IS USED.
5. THE ENGINEER OF RECORD SHALL REVIEW THE MAXIMUM BASE REACTIONS AND DESIGN WIND SPEED FOR THE UNIT TO BE INSTALLED TO DETERMINE IF THE FOUNDATION'S MAXIMUM DESIGN LOADS HAVE NOT BEEN EXCEEDED. THIS FOUNDATION DESIGN SHALL NOT BE USED IF THE MAXIMUM DESIGN LOADS OR WIND SPEED HAVE BEEN EXCEEDED.
6. THIS FOUNDATION DESIGN SHALL NOT BE USED IN LOCATIONS WHICH ARE CLOSER THAN 8ft FROM A RETAINING WALL.
7. THIS FOUNDATION DESIGN SHALL NOT BE USED AT LOCATIONS WHERE THE GROUND SLOPE EXCEEDS 4 INCHES PER FOOT.

**GENERAL NOTES:**

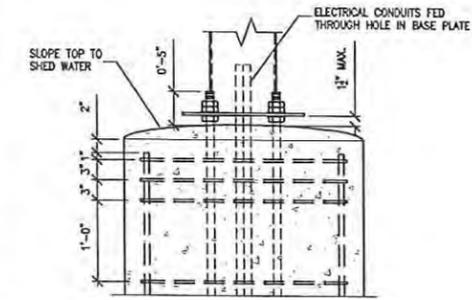
1. PIER DEPTHS SHOWN ARE REQUIRED MINIMUMS. ALL PIERS TO EXTEND TO FROST DEPTH AS DETERMINED BY LOCAL JURISDICTION.
2. TOP OF PIERS SHALL BE SLOPED SUCH THAT MOISTURE CANNOT ACCUMULATE.
3. TOP 6" OF SOIL NEGLECTED IN EMBEDMENT DEPTH CALCULATIONS, (EMBEDMENT DEPTHS SHOWN ARE FORM GRADE).
4. PROVIDE 3" MINIMUM CONCRETE COVER TO REINFORCING BARS, UNLESS NOTED OTHERWISE.
5. ALL REINFORCING BY GENERAL CONTRACTOR.
6. ELECTRICAL CONTRACTOR TO PROVIDE INFORMATION ON CONDUIT AND ELECTRICAL REQUIREMENTS.



**(A) FOUNDATION SECTION**  
3/4" = 1'-0"



**(B) ANCHOR BOLT DETAILS**  
1" = 1'-0"



**(C) CONNECTION DETAIL**  
1" = 1'-0"

**SINGLE WELCOME POINT GATEWAY FOUNDATION**

NO.	REVISIONS	DATE

Prepared For:

Lingle Design Group, Inc.  
158 West Main Street  
Lena, IL 61048  
**McDONALD'S - VERNON HILLS, IL**  
700 North Milwaukee Avenue  
Vernon Hills, Illinois

Prepared By:

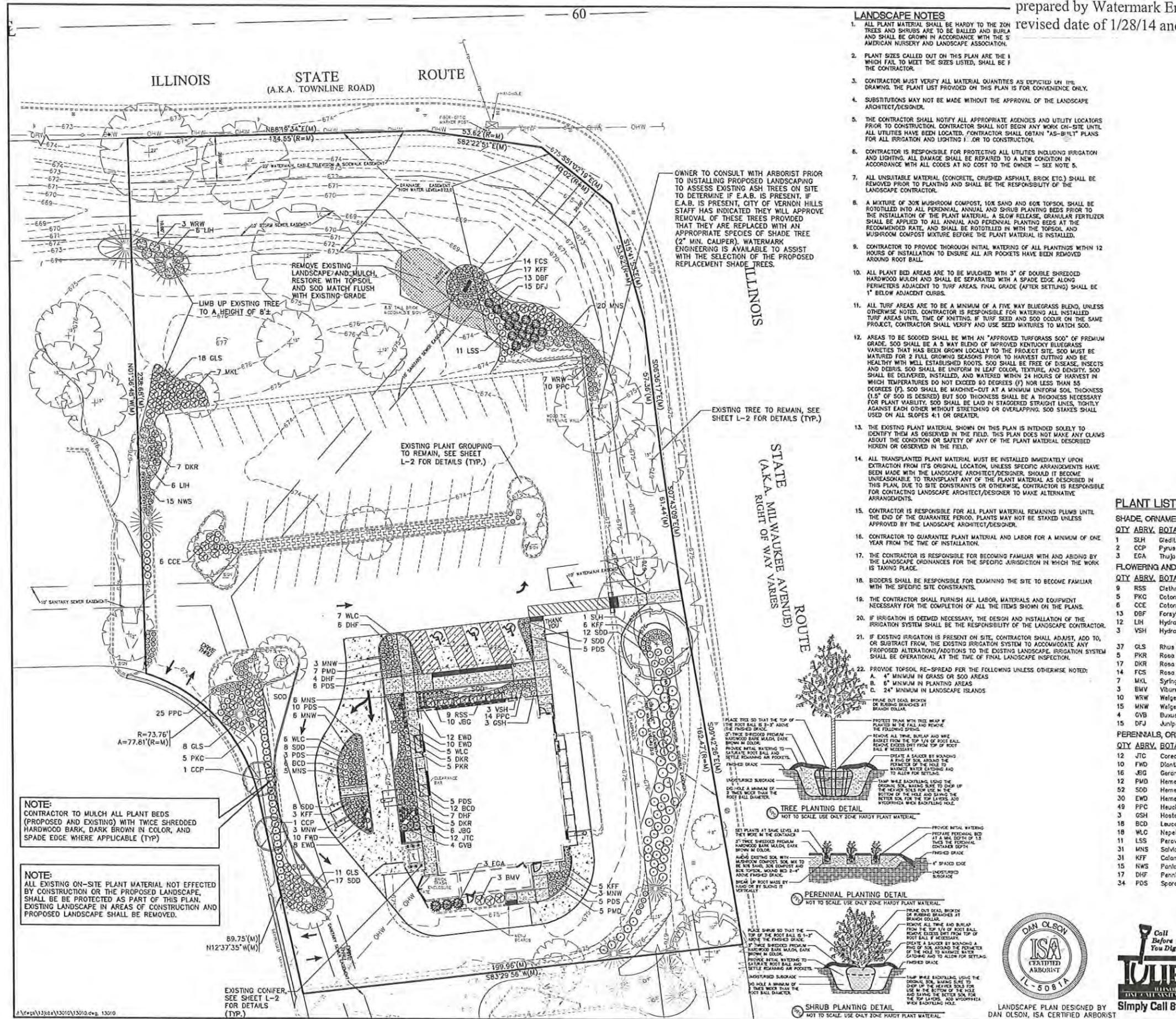
SAMANTANO AND COMPANY  
STRUCTURAL ENGINEERS  
221 NORTH LASALLE STREET  
CHICAGO, ILLINOIS 60601  
312.332.2320



CHECKED BY: JAS
DESIGN BY: SN
DRAWN BY: GM
DATE: MAY 30, 2013
SCALE: NOTED
PROJECT NO.: 13-010

**SSD-2**  
LC #12-1085

WELCOME POINT GATEWAY FOUNDATION



- ### LANDSCAPE NOTES
- ALL PLANT MATERIAL SHALL BE HARDY TO THE ZONE. TREES AND SHRUBS ARE TO BE BALLED AND BURLA AND SHALL BE GROWN IN ACCORDANCE WITH THE S.A. AMERICAN NURSERY AND LANDSCAPE ASSOCIATION.
  - PLANT SIZES CALLED OUT ON THIS PLAN ARE THE 1 WHICH FAIL TO MEET THE SIZES LISTED, SHALL BE F THE CONTRACTOR.
  - CONTRACTOR MUST VERIFY ALL MATERIAL QUANTITIES AS DEPICTED ON THE DRAWING. THE PLANT LIST PROVIDED ON THIS PLAN IS FOR CONVENIENCE ONLY.
  - SUBSTITUTIONS MAY NOT BE MADE WITHOUT THE APPROVAL OF THE LANDSCAPE ARCHITECT/DESIGNER.
  - THE CONTRACTOR SHALL NOTIFY ALL APPROPRIATE AGENCIES AND UTILITY LOCATORS PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOT BEGIN ANY WORK ON-SITE UNTIL ALL UTILITIES HAVE BEEN LOCATED. CONTRACTOR SHALL OBTAIN "AS-BUILT" PLANS FOR ALL IRRIGATION AND LIGHTING 1. OR TO CONSTRUCTION.
  - CONTRACTOR IS RESPONSIBLE FOR PROTECTING ALL UTILITIES INCLUDING IRRIGATION AND LIGHTING. ALL DAMAGE SHALL BE REPAIRED TO A NEW CONDITION IN ACCORDANCE WITH ALL CODES AT NO COST TO THE OWNER - SEE NOTE 5.
  - ALL UNSUITABLE MATERIAL (CONCRETE, CRUSHED ASPHALT, BRICK ETC) SHALL BE REMOVED PRIOR TO PLANTING AND SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
  - A MIXTURE OF 30% MUSHROOM COMPOST, 10% SAND AND 60% TOPSOIL SHALL BE ROTOTILLED INTO ALL PERENNIAL, ANNUAL AND SHRUB PLANTING BEDS PRIOR TO THE INSTALLATION OF THE PLANT MATERIAL. A SLOW RELEASE, GRANULAR FERTILIZER SHALL BE APPLIED TO ALL ANNUAL AND PERENNIAL PLANTING BEDS AT THE RECOMMENDED RATE AND SHALL BE ROTOTILLED WITH THE TOPSOIL AND MUSHROOM COMPOST MIXTURE BEFORE THE PLANT MATERIAL IS INSTALLED.
  - CONTRACTOR TO PROVIDE THOROUGH INITIAL WATERING OF ALL PLANTINGS WITHIN 12 HOURS OF INSTALLATION TO ENSURE ALL AIR POCKETS HAVE BEEN REMOVED AROUND ROOT BALL.
  - ALL PLANT BED AREAS ARE TO BE MULCHED WITH 3" OF DOUBLE SHREDED HARDWOOD MULCH AND SHALL BE SEPARATED WITH A SPADE EDGE ALONG PERIMETERS ADJACENT TO TURF AREAS. FINAL GRADE (AFTER SETTLING) SHALL BE 1" BELOW ADJACENT CURBS.
  - ALL TURF AREAS ARE TO BE A MINIMUM OF A FIVE WAY BLUEGRASS BLEND, UNLESS OTHERWISE NOTED. CONTRACTOR IS RESPONSIBLE FOR WATERING ALL INSTALLED TURF AREAS UNTIL TIME OF KNOTTING. IF TURF SEED AND SOO OCCUR ON THE SAME PROJECT, CONTRACTOR SHALL VERIFY AND USE SEED MIXTURES TO MATCH SOO.
  - AREAS TO BE SOODED SHALL BE WITH AN "APPROVED TURFGRASS SOO" OF PREMIUM GRADE. SOO SHALL BE A 5 WAY BLEND OF IMPROVED KENTUCKY BLUEGRASS VARIETIES THAT HAS BEEN GROWN LOCALLY TO THE PROJECT SITE. SOO MUST BE MATURED FOR 2 FULL GROWING SEASONS PRIOR TO HARVEST CUTTING AND BE HEALTHY WITH WELL ESTABLISHED ROOTS. SOO SHALL BE FREE OF DISEASE, INSECTS AND DEBRIS. SOO SHALL BE UNIFORM IN LEAF COLOR, TEXTURE, AND DENSITY. SOO SHALL BE DELIVERED, INSTALLED, AND WATERED WITHIN 24 HOURS OF HARVEST IN WHICH TEMPERATURES DO NOT EXCEED 80 DEGREES (F) NOR LESS THAN 55 DEGREES (F). SOO SHALL BE MACHINED-OUT AT A MINIMUM UNIFORM SOO THICKNESS (1/2" OF SOO IS DESIRED) BUT SOO THICKNESS SHALL BE A THICKNESS NECESSARY FOR PLANT VIABILITY. SOO SHALL BE LAID IN STAGGERED STRAIGHT LINES, TIGHTLY AGAINST EACH OTHER WITHOUT STRETCHING OR OVERLAPPING. SOO STACKS SHALL USED ON ALL SLOPES 4:1 OR GREATER.
  - THE EXISTING PLANT MATERIAL SHOWN ON THIS PLAN IS INTENDED SOLELY TO IDENTIFY THEM AS OBSERVED IN THE FIELD. THIS PLAN DOES NOT MAKE ANY CLAIMS ABOUT THE CONDITION OR SAFETY OF ANY OF THE PLANT MATERIAL DESCRIBED HEREIN OR OBSERVED IN THE FIELD.
  - ALL TRANSPLANTED PLANT MATERIAL MUST BE INSTALLED IMMEDIATELY UPON EXTRACTION FROM ITS ORIGINAL LOCATION, UNLESS SPECIFIC ARRANGEMENTS HAVE BEEN MADE WITH THE LANDSCAPE ARCHITECT/DESIGNER. SHOULD IT BECOME UNREASONABLE TO TRANSPORT ANY OF THE PLANT MATERIAL AS DESCRIBED IN THIS PLAN, DUE TO SITE CONSTRAINTS OR OTHERWISE, CONTRACTOR IS RESPONSIBLE FOR CONTACTING LANDSCAPE ARCHITECT/DESIGNER TO MAKE ALTERNATIVE ARRANGEMENTS.
  - CONTRACTOR IS RESPONSIBLE FOR ALL PLANT MATERIAL REMAINING PLUMS UNTIL THE END OF THE GUARANTEE PERIOD. PLANTS MAY NOT BE STAKED UNLESS APPROVED BY THE LANDSCAPE ARCHITECT/DESIGNER.
  - CONTRACTOR TO GUARANTEE PLANT MATERIAL AND LABOR FOR A MINIMUM OF ONE YEAR FROM THE TIME OF INSTALLATION.
  - THE CONTRACTOR IS RESPONSIBLE FOR BECOMING FAMILIAR WITH AND ABIDING BY THE LANDSCAPE ORDINANCES FOR THE SPECIFIC JURISDICTION IN WHICH THE WORK IS TAKING PLACE.
  - BIDDERS SHALL BE RESPONSIBLE FOR EXAMINING THE SITE TO BECOME FAMILIAR WITH THE SPECIFIC SITE CONSTRAINTS.
  - THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY FOR THE COMPLETION OF ALL THE ITEMS SHOWN ON THE PLANS.
  - IF IRRIGATION IS DEEMED NECESSARY, THE DESIGN AND INSTALLATION OF THE IRRIGATION SYSTEM SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
  - IF EXISTING IRRIGATION IS PRESENT ON SITE, CONTRACTOR SHALL ADJUST, ADD TO, OR SUSPECT FROM THE EXISTING IRRIGATION SYSTEM TO ACCOMMODATE ANY PROPOSED ALTERATIONS/ADDITIONS TO THE EXISTING LANDSCAPE. IRRIGATION SYSTEM SHALL BE OPERATIONAL AT THE TIME OF FINAL LANDSCAPE INSPECTION.
  - PROVIDE TOPSOIL RE-SREAD PER THE FOLLOWING UNLESS OTHERWISE NOTED:  
 A. 4" MINIMUM IN GRASS OR SOO AREAS  
 B. 6" MINIMUM IN PLANTING AREAS  
 C. 24" MINIMUM IN LANDSCAPE ISLANDS

ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.  
 3. NO PROPOSED MODIFICATION TO SITE LIGHTING.  
 4. PER FIRE CODE REQUIREMENTS ACCESS MUST BE MAINTAINED TO THE BUILDING DURING THE CONSTRUCTION.

### SITE DATA

LOT AREA =	82,995 S.F.(1.90 AC.)
EXISTING BUILDING AREA =	4,776 S.F.
EXISTING BUILDING SEATING =	80 SEATS
EMPLOYEES PER SHIFT =	12 EMPLOYEES
F.A.R. =	0.06
EXISTING PERVIOUS AREA =	37,647 S.F.(0.86 AC.)(45.3%)
EXISTING IMPERVIOUS AREA =	45,348 S.F.(1.04 AC.)(54.7%)
PROPOSED PERVIOUS AREA =	38,098 S.F.(0.87 AC.)(45.5%)
PROPOSED IMPERVIOUS AREA =	44,897 S.F.(1.03 AC.)(54.5%)

NOTE: NO CHANGES TO THE EXISTING DETENTION BASIN VOLUME ARE PROPOSED AS PART OF THIS PROJECT.

### ON SITE PARKING DATA

EXISTING REGULAR SPACES	59
EXISTING ADA ACCESSIBLE SPACES PROVIDED	2
TOTAL EXISTING SPACES	61
PROPOSED REGULAR SPACES	48
PROPOSED ADA ACCESSIBLE SPACES PROVIDED	3
TOTAL PROPOSED SPACES	51

TOTAL SPACES REQUIRED = 1 SPACE/2 SEATS + 1 SPACE/2 EMPLOYEES.  
 TOTAL SPACES REQUIRED = 80/2 + 12/2 = 46

51 PROPOSED SPACES > 46 REQUIRED SPACES

### DRIVE THRU STACKING DATA

NOT REQUIRED

### BICYCLE PARKING DATA

NOT REQUIRED

### PLANT LIST

QTY	ABRV.	BOTANICAL NAME	COMMON NAME	SIZE
1	SLH	Gleditsia triacanthos var. inermis 'Skyline'	Skyline Honeylocust	3"
2	CCP	Pyrus calleryana 'Chanticleer'	Chanticleer Pear	3"
3	ECA	Thuja occidentalis 'Emerald'	Emerald Green Arborvitae	6'

### FLOWERING AND EVERGREEN SHRUBS

QTY	ABRV.	BOTANICAL NAME	COMMON NAME	SIZE
9	RSS	Clethra alnifolia 'Ruby Spice'	Ruby Spice Summerweet	5 Gal.
5	PKC	Cotoneaster 'Acutifolius'	Peking Cotoneaster	5 Gal.
6	CCE	Cotoneaster 'Apiculatus'	Cranberry Cotoneaster	5 Gal.
13	DBF	Forsythia viridissima 'Bronxensis'	Dwarf Bronx Forsythia	5 Gal.
12	LH	Hydrangea paniculata 'Limelight'	Limelight Hydrangea	5 Gal.
3	VSH	Hydrangea paniculata 'Renhy'	First Edition Vanilla Strawberry Hydrangea	5 Gal.

### PERENNIALS, ORNAMENTAL GRASS AND GROUNDCOVERS

QTY	ABRV.	BOTANICAL NAME	COMMON NAME	SIZE
12	JTC	Coreopsis 'Jethro Bull'	Jethro Bull Coreopsis	1 Gal.
10	FWD	Dianthus gratianopolitanus 'Firewitch'	Firewitch Dianthus	1 Gal.
12	JBG	Geronium 'Johnson's Blue'	Johnson's Blue Geronium	1 Gal.
16	PMD	Hemerocallis 'Pardon Me'	Pardon Me Daylily	1 Gal.
52	SDD	Hemerocallis 'Stella de Oro'	Stella de Oro Daylily	1 Gal.
20	EWO	Hemerocallis x 'Eenie-Weenie'	Eenie-Weenie Daylily	1 Gal.
49	PPC	Heuchera micrantha 'Palace Purple'	Palace Purple Coral Bells	1 Gal.
3	GSH	Hosta 'Gold Standard'	Gold Standard Hosta	1 Gal.
18	BCD	Leucanthemum x superbum 'Banana Cream'	Banana Cream Shasta Daisy	1 Gal.
18	WLC	Nepeta racemosa 'Walker's Low'	Walker's Low Catmint	1 Gal.
11	LSS	Perovskia atriplicifolia 'Little Spire'	Little Spire Russian Sage	1 Gal.
31	MNS	Salvia nemorosa 'May Night'	May Night Salvia	1 Gal.
31	KFF	Calamagrostis x acutiflora 'Karl Foerster'	Karl Foerster Feather Reed Grass	3 Gal.
15	NWS	Panicum virgatum 'Northwind'	Northwind Switch Grass	1 Gal.
17	DHF	Pennisetum alopecuroides 'Hornet'	Dwarf Hornet Fountain Grass	1 Gal.
34	PDS	Sparganium heterotepes	Prairie Dropseed	1 Gal.

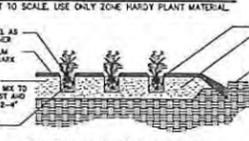
NOTE: CONTRACTOR TO MULCH ALL PLANT BEDS (PROPOSED AND EXISTING) WITH TWICE SHREDED HARDWOOD BARK, DARK BROWN IN COLOR, AND SPADE EDGE WHERE APPLICABLE (TYP.)

NOTE: ALL EXISTING ON-SITE PLANT MATERIAL NOT EFFECTED BY CONSTRUCTION OR THE PROPOSED LANDSCAPE, SHALL BE PROTECTED AS PART OF THIS PLAN. EXISTING LANDSCAPE IN AREAS OF CONSTRUCTION AND PROPOSED LANDSCAPE SHALL BE REMOVED.

OWNER TO CONSULT WITH ARBORIST PRIOR TO INSTALLING PROPOSED LANDSCAPING TO ASSESS EXISTING ASH TREES ON SITE TO DETERMINE IF E.A.B. IS PRESENT. IF E.A.B. IS PRESENT, CITY OF VERNON HILLS STAFF HAS INDICATED THEY WILL APPROVE REMOVAL OF THESE TREES PROVIDED THAT THEY ARE REPLACED WITH AN APPROPRIATE SPECIES OF SHADE TREE (2" MIN. CALIPER). WATERMARK ENGINEERING IS AVAILABLE TO ASSIST WITH THE SELECTION OF THE PROPOSED REPLACEMENT SHADE TREES.

EXISTING TREE TO REMAIN, SEE SHEET L-2 FOR DETAILS (TYP.)

EXISTING PLANT GROUPING TO REMAIN, SEE SHEET L-2 FOR DETAILS (TYP.)



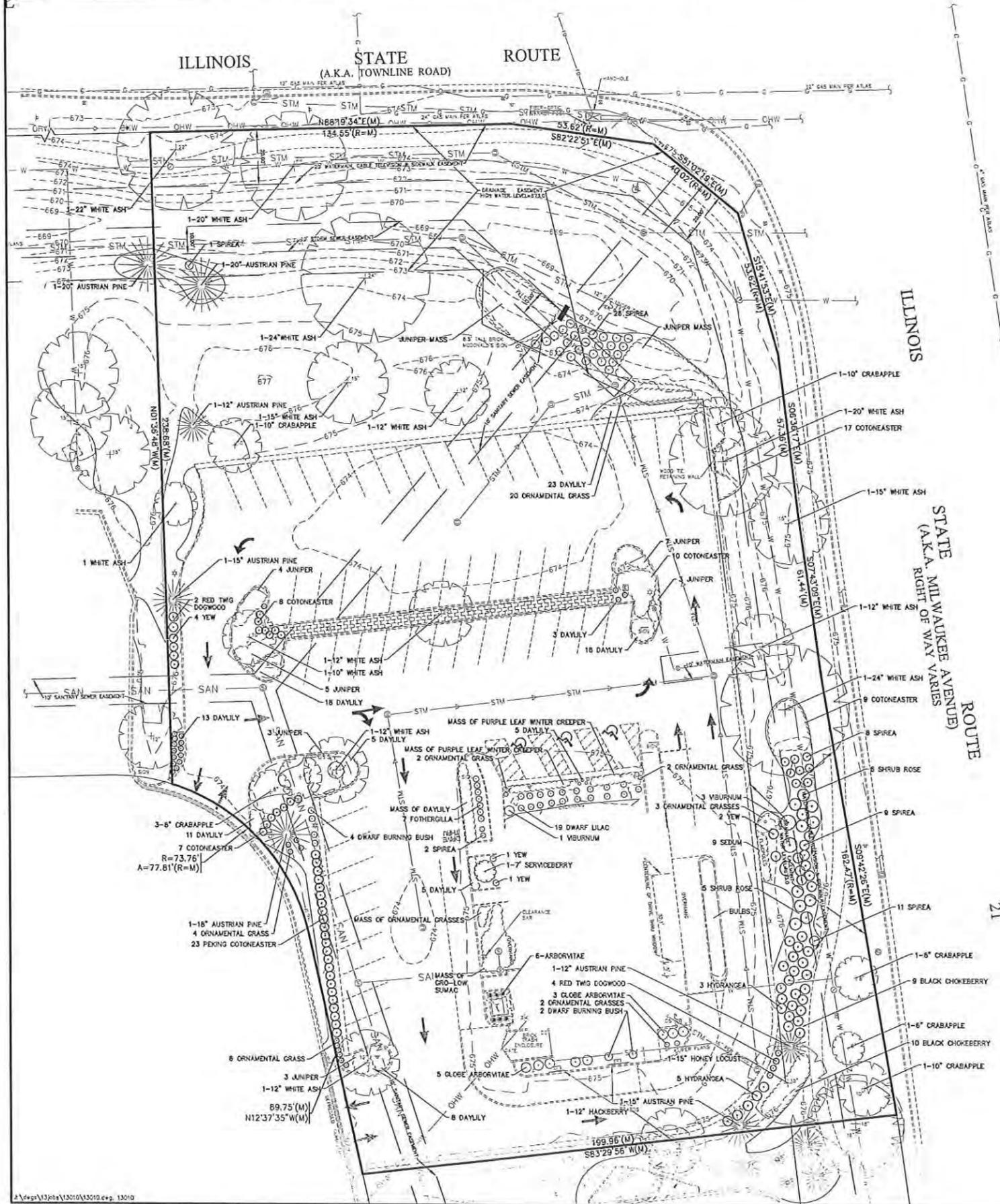
## LANDSCAPE PLAN

CHECKED BY: J. MILLER	DESIGN BY: M.BAILEY	DRAWN BY: K. SACK	DATE: NOVEMBER 21, 2013	SCALE: 1" = 20'	PROJECT NO: 13-010
NO REVISIONS	NO REVISIONS	NO REVISIONS	NO REVISIONS	NO REVISIONS	NO REVISIONS
DATE: 01.09.14	DATE: 01.20.14	DATE: 01.20.14	DATE: 01.20.14	DATE: 01.20.14	DATE: 01.20.14

Prepared For:  
**McDonald's - VERNON HILLS, IL**  
 700 N. Milwaukee Avenue  
 Vernon Hills, Illinois

Prepared By:  
**Watermark Engineering Resources, LTD**  
 2031 Clinger Woods Parkway, Suite 100, Aurora, IL 60502  
 phone 630-575-1600 fax 630-575-0800 www.watermark-engineering.com

LANDSCAPE PLAN  
 L-1  
 LC #12-1085



**GENERAL NOTES:**

1. THESE PLANS ARE BASED ON THE BOUNDARY AND TOPOGRAPHIC SURVEY (SURVEY PROJECT #13.0016 DATED 01/31/13) PREPARED BY: COMPASS SURVEYING LTD 2631 GINGER WOODS PARKWAY, STE 100, AURORA, IL 60502 (630) 820-9100
2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.
3. NO PROPOSED MODIFICATION TO SITE LIGHTING.
4. PER FIRE CODE REQUIREMENTS ACCESS MUST BE MAINTAINED TO THE BUILDING DURING THE CONSTRUCTION.

1. THIS PLAN IS INTENDED SOLELY TO IDENTIFY THE EXISTING PLANT MATERIAL AS OBSERVED IN THE FIELD. THIS PLAN DOES NOT MAKE ANY CLAIMS ABOUT THE CONDITION OR SAFETY OF ANY OF THE PLANT MATERIAL DESCRIBED HEREIN OR OBSERVED IN THE FIELD.

2. ALL EXISTING PLANT MATERIAL, NOT SHOWN ON THE SURVEY PROVIDED, WERE LOCATED IN THE FIELD BY THE LANDSCAPE DESIGNER AND THEREFORE ARE APPROXIMATE IN THEIR LOCATION AS SHOWN ON THIS PLAN.

NO.	DATE	REVISIONS
1	01/31/13	NO REVISIONS
2	01/25/14	NO REVISIONS

Prepared For:

**Lingle Design Group**  
 158 W. Main Street  
 Lena, IL 61048

**McDonald's - VERNON HILLS, IL**  
 700 N. Milwaukee Avenue  
 Vernon Hills, Illinois

Prepared By:

**Watermark Engineering Resources, Ltd.**  
 2631 Ginger Woods Parkway  
 Aurora, IL 60502  
 phone 630-375-1000 fax 630-208-0850 www.watermark-engineering.com



AS-BUILT LANDSCAPE PLAN PREPARED BY DAN OLSON, ISA CERTIFIED ARBORIST



**AS-BUILT LANDSCAPE PLAN**

CHECKED BY: J. MILLER  
 DESIGN BY: M. BAILEY  
 DRAWN BY: K. SACK  
 DATE: NOVEMBER 21, 2013  
 SCALE: 1" = 20'  
 PROJECT NO.: 13-010

**L-2**  
 LC #12-1085

AS-BUILT LANDSCAPE PLAN





Site Perspective



IMAGINATE



McDonald's  
700 N. Milwaukee  
Vernon Hills, IL



Site Perspective



IMAGINATE

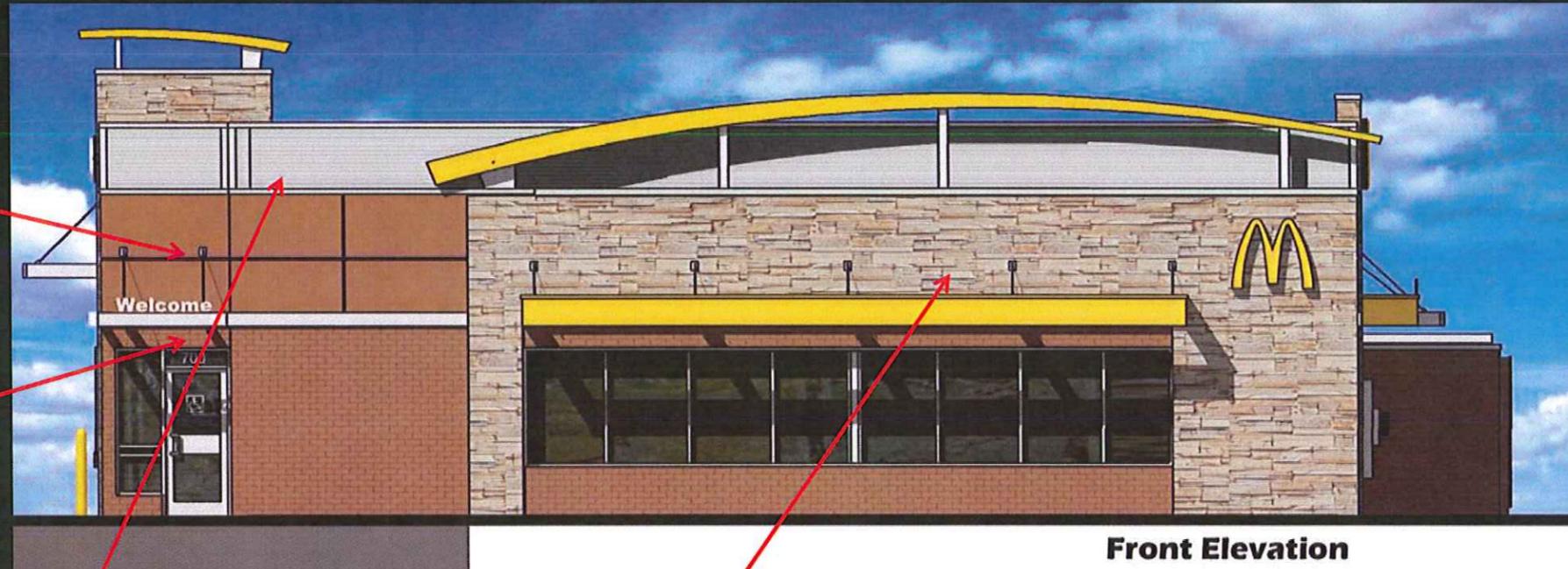


McDonald's  
700 N. Milwaukee  
Vernon Hills, IL

# Material Board

Painted EFIS  
Coyote Trail  
BM-1224

Painted Brick  
Coyote Trail  
BM-1224



Front Elevation

Metal Corrugated 7/8" "S"



24 GA Color: City Scape

Southwest Ledge Stone

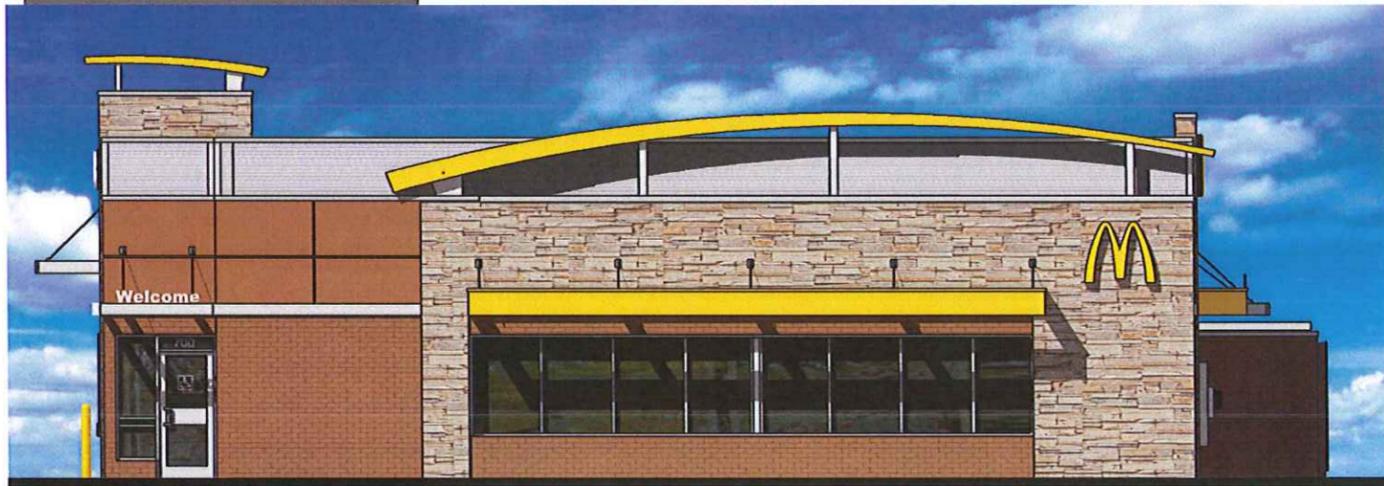


Cultured Stone by Boral

Painted Brick Potters Clay BM-1221



Drive-Thru Elevation



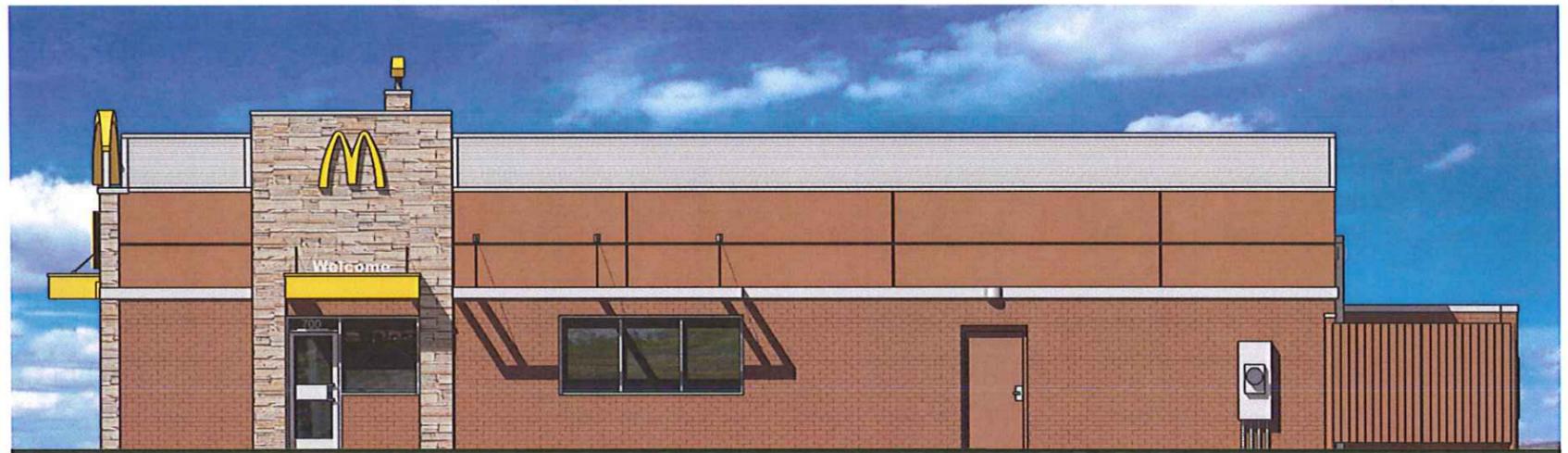
**Front Elevation**



**Rear Elevation**

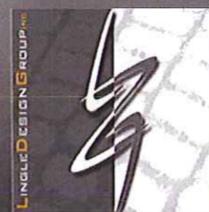


**Drive-Thru Elevation**



**Non-Drive-Thru Elevation**

**Existing Images**

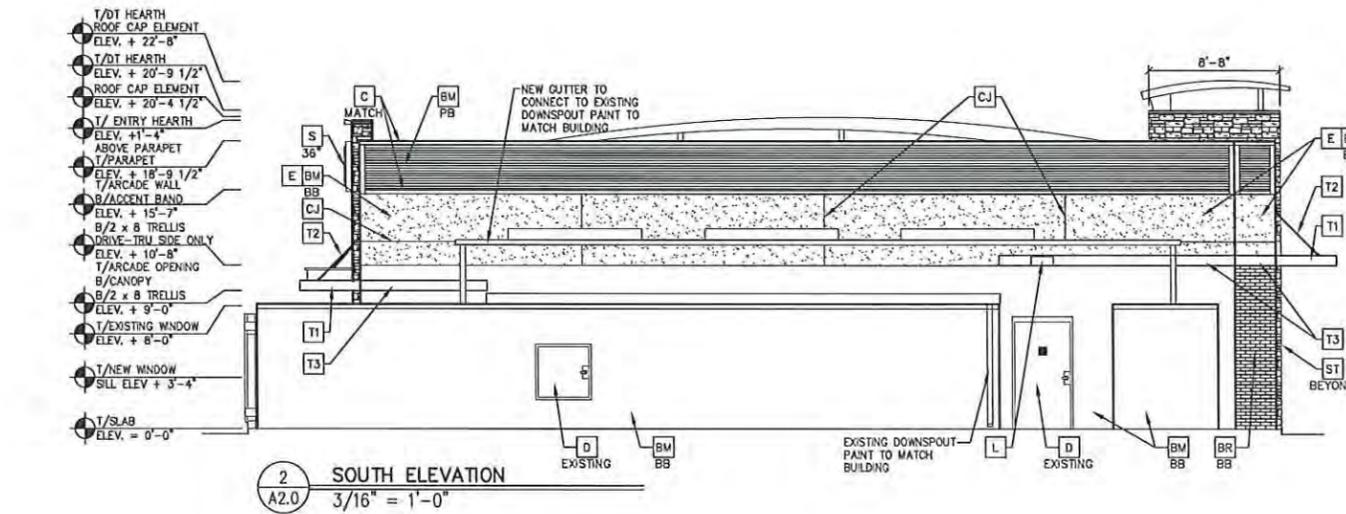
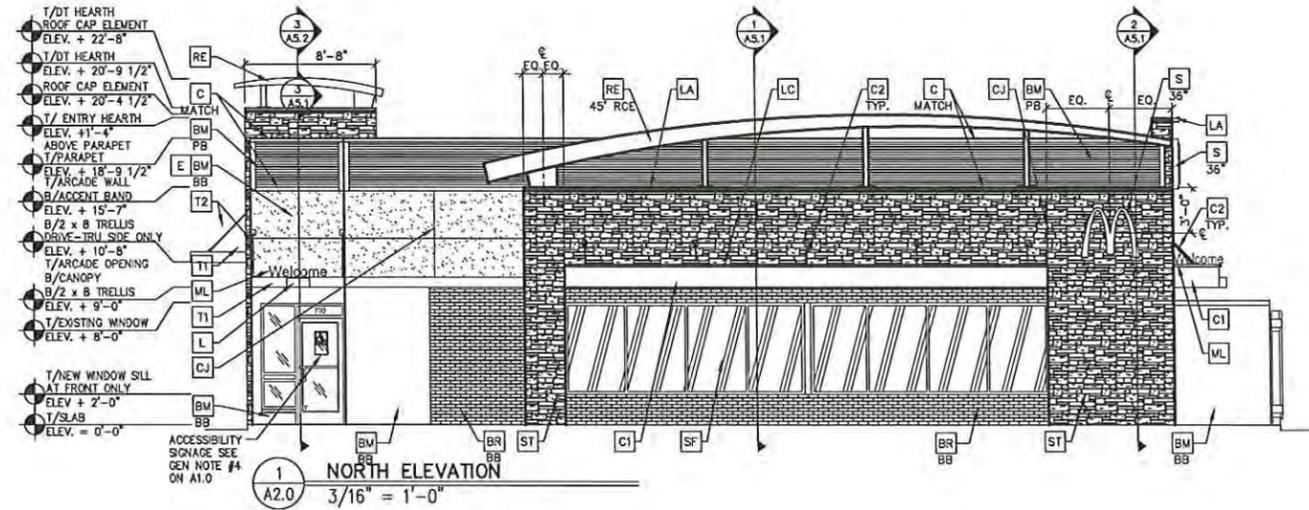


**IMAGINATE**

Imagine Create Excite



**McDonald's**  
700 N. Milwaukee  
Vernon Hills, IL



**KEY NOTES:**

- A** METAL FINISH - UNDER SEPARATE PERMIT, COLOR TO BE ALTERNATING STRIPES TO MATCH PANTONE 123U AND 109U
- BM** BUILDING MATERIAL
  - COLOR:
  - BB = BASE BUILDING
  - DT = DRIVE THRU BANDING
  - PB = PARAPET BAND
  - AH = ARCADE AND HEARTH
- BR** FACEBRICK
  - COLOR:
  - BB = BASE BUILDING
  - DT = DRIVE THRU BANDING
- C1** NEW ALUMINUM CANOPY BY OTHERS, SEE ELEVATIONS (COLOR: YELLOW)
- C2** NEW ALUMINUM CANOPY TIE-BACK BY OTHERS, SEE ELEVATIONS
- C** METAL COPING - COLOR TO MATCH SURROUNDING MATERIAL
- CJ** CONTROL JOINT
- CS** OUTSIDE CORNER STRIP BY SCHLUTER - SEE TILE PACKAGE AND GENERAL NOTES FOR ADDITIONAL INFORMATION
- CT** CERAMIC TILE - 12" X 24" BY EUROWEST
- D** HOLLOW METAL DOOR - PAINT TO MATCH COLOR OF SURROUNDING MATERIAL
- E** EXT. INSULATION FINISH SYSTEM (EIF.S.)
- FB** CO2 = CO2 FILL BOX (EOPM SCHEDULE ITEM 49.00)  
BO = OPTIONAL BLANK OIL FILL BOX (EOPM SCHEDULE ITEM 700.1B) CONFIRM USE WITH MCD AREA PROJECT MANAGER
- GR** GUARDRAIL
- L** LIGHT FIXTURE (WALL SCONCE) - SEE ELECTRICAL
- LA** LIGHT FIXTURE (ARCADE LIGHT) - SEE ELECTRICAL
- LC** LIGHT FIXTURE (CANOPY LIGHT) - SEE ELECTRICAL
- ML** METAL LETTERING - BY OTHERS
- MJ** MOVEMENT JOINT BY SCHLUTER - SEE TILE PACKAGE AND GENERAL NOTES FOR ADDITIONAL INFORMATION
- PB** PIPE BOLLARD - PAINTED YELLOW
- PT** PASS-THRU COIN COLLECTOR - OPTIONAL (RMHC)
- RE** ROOF CAP ELEMENT BY OTHERS
- RL** ROOF LADDER (EXISTING)
- RO** ROOF DRAIN OVERFLOW PIPE PAINT TO MATCH SURROUNDING MATERIAL
- S** MCDONALD'S SIGNAGE BY OTHERS - UNDER SEPARATE PERMIT, SEE SIGNAGE SHOP DRAWINGS FOR SUPPORT NOTES.
- SF** NEW STOREFRONT AND GLAZING
- T** TEMPERED GLASS
- SP** SPANDREL PANEL COLOR TO MATCH SURROUNDING MATERIAL
- SS** METAL LOUVER SUNSCREEN PANEL
- ST** CULTURED STONE VENEER BY BORAL STONE PRODUCTS
- TI** ALUMINUM TRELLIS
- T2** ALUMINUM TRELLIS TIE-BACK
- T3** ALUMINUM TRELLIS 2x8 WALL FASCIA
- TB** THIN BRICK
  - COLOR:
  - B1 = BASE BUILDING
  - B2 = DRIVE THRU BANDING
- U** SLOPPER AND DOWNSPOUT SEE DETAIL 11/AA.1
- W2** DRIVE-THRU WINDOW BY READY ACCESS CONFIRM MODEL, OPTIONS, AND SIZE WITH MCD PROJECT MANAGER OPTIONS INCLUDE: TRANSOM (SHOWN)
- XX** SLIDE DIRECTION: RL = RIGHT TO LEFT  
LR = LEFT TO RIGHT

**NOTE:**

1. EIFS AND BRICK VENEER CONTROL JOINTS INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
2. TRASH CORRAL COLOR TO MATCH MAIN BUILDING.
3. BOTTOM DRIVE THRU COLOR BAND SHOULD BE DARKER COLOR OF ANY EXTERIOR TWO COLOR SCHEME.
4. TOP OF DRIVE THRU BAND HEIGHT WILL VARY ACCORDING TO BOTTOM OF TRELLIS LOCATION.

**NOTE:**

**COLOR LEGEND** (ALL COLORS ARE PER BENJAMIN MOORE PAINTS, U.M.O.)

COYOTE BROWN SCHEME	ARCADIES & HEARTH	SOUTHWEST PRO-FIT LEDGESTONE PF-8019
	BASE BUILDING	COYOTE TRAIL BM-1224
	DRIVE THRU BANDING	POTTERS CLAY BM-1221
	PARAPET BAND	CORRUGATED METAL - 8" 5" PANEL 24 GAUGE COLOR: CITY SCAPE

NOTE: NEW BRICK TO MATCH EXISTING SIZE AND TEXTURE.

UNIQUEDESIGNGROUP, INC.  
136 WEST MAIN STREET  
LENA, IL 61848 158  
1860 W. EVANG AVE.  
ENCLWOOD, CO 80110  
PHONE: 815-369-1155  
FAX: 815-369-4495  
WWW.UNIQUEDESIGN.COM



ALL DRAWINGS AND SPECIFICATIONS, CONTRACTS AND THE PROPERTY OF THE UNIQUE DESIGN GROUP, INC. FOR ANY AND ALL PURPOSES. NO REUSE, REPRODUCTION, OR TRANSMISSION OF ANY INFORMATION FROM THIS DRAWING.



**McDonald's USA, LLC**  
700 NORTH MILWAUKEE  
VERNON HILLS, IL

REVISIONS:	DATE:	DESCRIPTION:

PROJECT NUMBER: 12-291  
DRAWN BY: TCM  
CHECKED BY: EC

ISSUED DATE: 03/21/2014

**EXTERIOR ELEVATIONS**

SHEET NUMBER:

**A2.0**

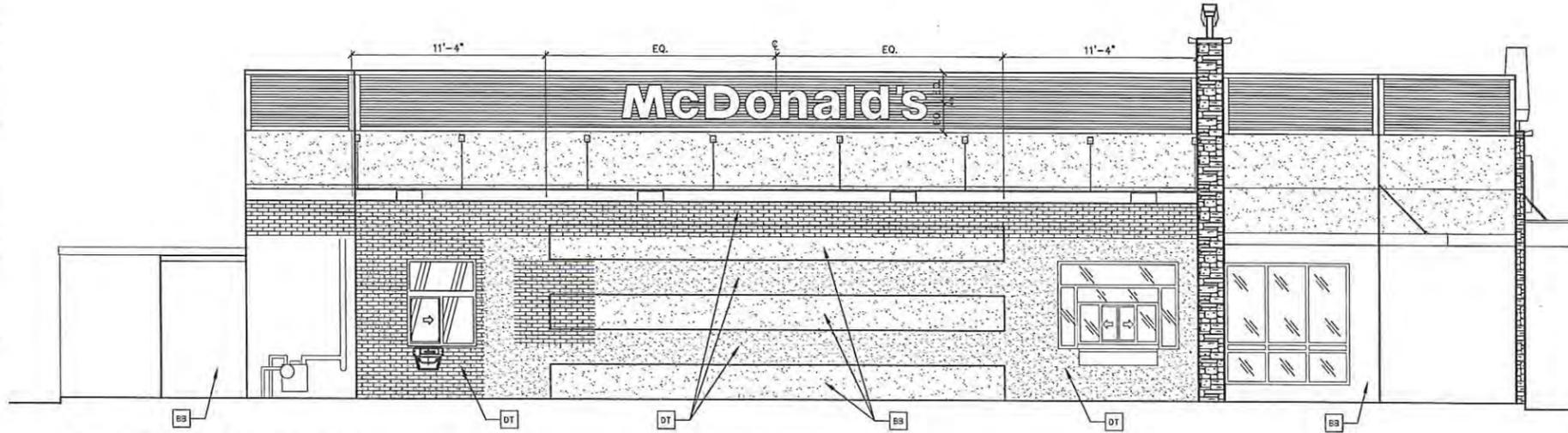
SIGN CODE ALLOWS 1 SQ FOOT PER LINER FOOT OF BUILDING FRONTAGE = 88 S.F. (MAX NOT TO EXCEED 150 S.F.)

**McDonald's**

ALLOWED: 88 S.F.  
REQUESTED: 32.83 S.F.

NO VARIANCE REQUIRED

- 1/DT HEARTH ROOF CAP ELEMENT ELEV. + 23'-4"
- 1/DT HEARTH ELEV. + 21'-4"
- ROOF CAP ELEMENT ELEV. + 20'-4"
- 1/PARAPET ELEV. + 18'-9 1/2"
- 1/ARCADE WALL 8/ACCENT BAND ELEV. + 15'-7"
- 8/2 x 8 TRELLIS DRIVE-THRU SIDE ONLY ELEV. + 11'-4"
- 1/ARCADE OPENING 8/CANOPY 8/2 x 8 TRELLIS ELEV. + 9'-0"
- 1/EXISTING WINDOW ELEV. + 7'-11 1/2"
- 1/NEW WINDOW SILL ELEV. + 3'-3"
- 1/SLAB ELEV. = 0'-0"



1 PROPOSED EAST ELEVATION  
A2.1 1/4" = 1'-0"

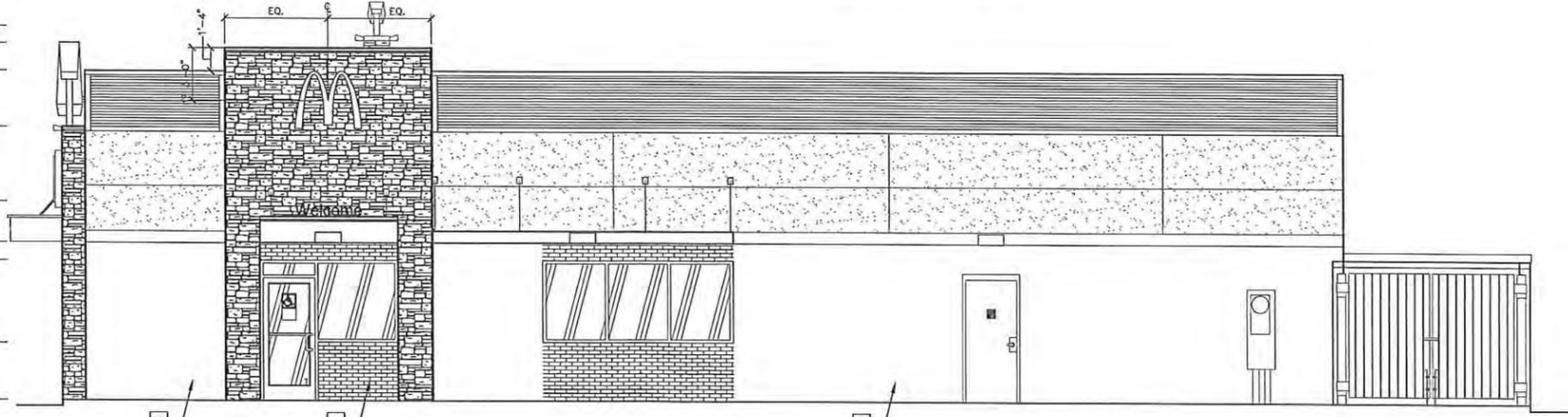
ASSUMING FACING THE RING ROAD IS DEEMED STREET FRONTAGE, ALLOWED 1 SIGN AT 44 S.F. (MAX NOT TO EXCEED 150 S.F.)

REQUESTING 1 SIGN AT 10.75 S.F.



LOGO CODE ALLOWED: 4 S.F.  
REQUESTED: 10.75 S.F.  
VARIATION TO ADD ARCH: 6.75 S.F.

- 1/DT HEARTH ROOF CAP ELEMENT ELEV. + 23'-4"
- 1/DT HEARTH ELEV. + 21'-4"
- ROOF CAP ELEMENT ELEV. + 20'-4"
- 1/PARAPET ELEV. + 18'-9 1/2"
- 1/ARCADE WALL 8/ACCENT BAND ELEV. + 15'-7"
- 8/2 x 8 TRELLIS DRIVE-THRU SIDE ONLY ELEV. + 11'-4"
- 1/ARCADE OPENING 8/CANOPY 8/2 x 8 TRELLIS ELEV. + 9'-0"
- 1/EXISTING WINDOW ELEV. + 7'-11 1/2"
- 1/NEW WINDOW SILL ELEV. + 3'-3"
- 1/SLAB ELEV. = 0'-0"



2 PROPOSED WEST ELEVATION  
A2.1 1/4" = 1'-0"

NOTE:  
COLOR LEGEND (ALL COLORS ARE PER BENJAMIN MOORE PAINTS, U.N.O.)

COYOTE BROWN	ARCADES & HEARTH	CULTURED STONE - MCO PFSW BLEND
SO-DME	BASE BUILDING	COYOTE TRAIL EM-1224
	DRIVE THRU BANDING	POTTERS CLAY EM-1221
	PARAPET BAND	CORRUGATED METAL - 1/2" PANEL 24 GAUGE COLOR: CITY SCAPE

UNIQUE DESIGN GROUP  
158 WEST MAIN ST., LENEXA, IL 61548  
P: 618-369-9100 F: 618-369-4499  
WWW.UNIQUEDESIGNGROUP.COM

CALL BEFORE ANY ACTION. MATERIAL CONTAINS  
NOTES AND THE PERSONS OF WHOM THERE IS A  
DISCREPANCY MAY NOT BE REVISED. CORRECT, REISSUE, OR  
REVISIONS IN ANY MANNER WITHOUT THE ARCHITECT'S  
AUTHORIZATION FROM THE ARCHITECT.

PREPARED FOR:  
**McDonald's USA, LLC**  
4320 WINFIELD ROAD  
WARRENVILLE, IL 60555



DATE: FEB 21, 2013

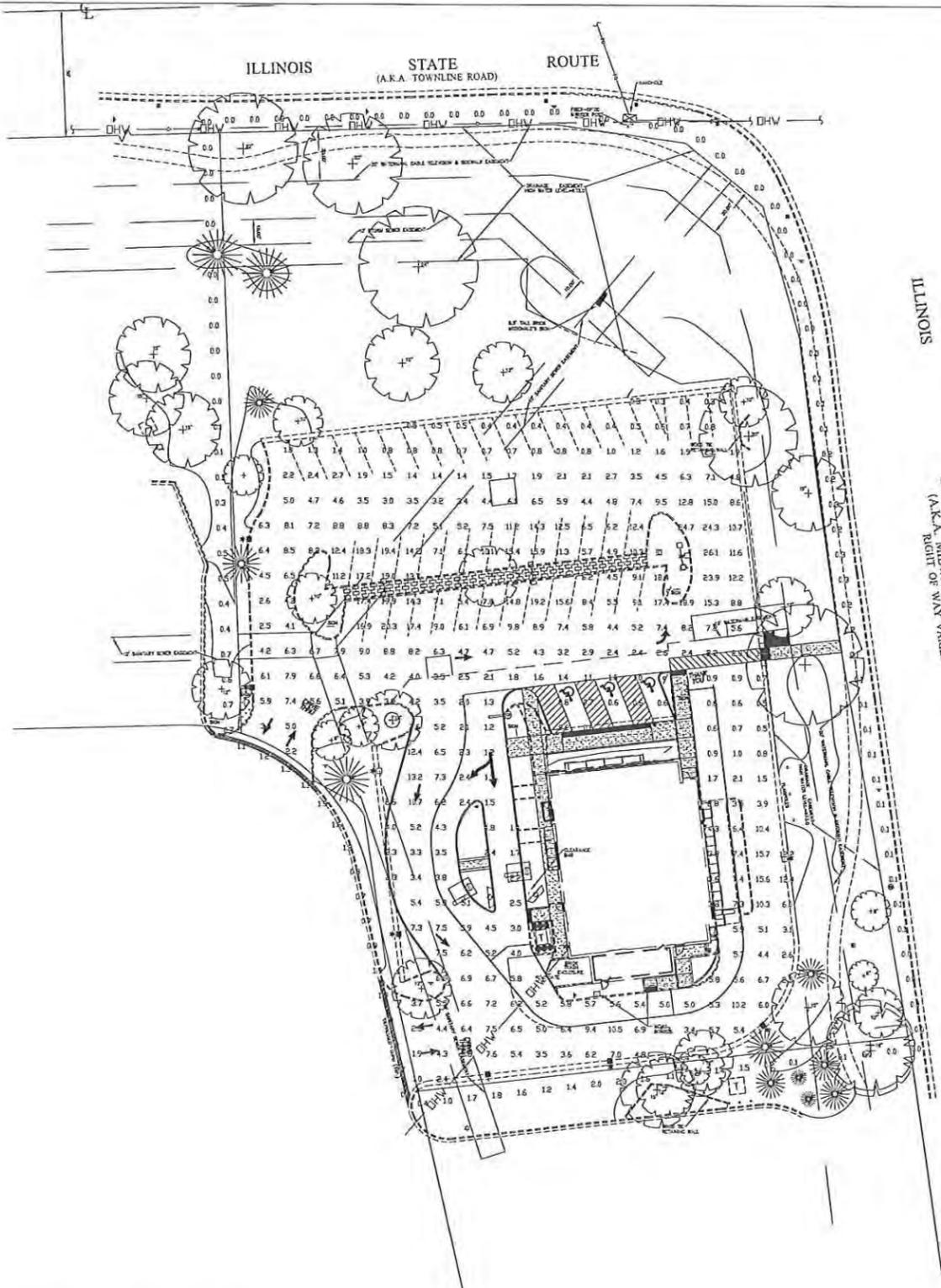
NO.	DATE	REVISIONS DESCRIPTION

McDONALD'S RESTAURANT  
700 NORTH MILLWALKEE  
VERNON HILLS, IL

DRAWN: DM  
CHECKED: FTM  
REVISED: FTM  
APPROVED: FTM

PROJ. NO.: XX-XXX  
DATE: 02/21/13  
SHEET NUMBER  
**A2.1**

- NOTES:
1. THE FOOTCANDLE LEVELS AS SHOWN ARE BASED ON THE FOLLOWING CRITERIA. ANY SUBSTITUTIONS IN SPECIFIED FIXTURES OR CHANGES TO LAYOUT WILL AFFECT LIGHTING LEVELS SHOWN AND WILL NOT BE THE RESPONSIBILITY OF SECURITY LIGHTING.
  2. THE CONTRIBUTION OF THE SOFFIT / BUILDING LIGHTING IS NOT REFLECTED ON THE DRAWING UNLESS SOFFIT / BUILDING LIGHTING IS SPECIFIED IN THE FIXTURE SCHEDULE BELOW.
  3. DISTANCE BETWEEN READINGS 10'
  4. FINAL ADJUSTMENTS TO AIMING ANGLE/DIRECTION OF FIXTURES MAY BE REQUIRED TO ELIMINATE LIGHT TRESPASS OR GLARE INTO ADJOINING PROPERTIES OR ROADWAYS.
  5. FOOTCANDLE LEVELS SHOWN ARE MAINTAINED. MAINTENANCE FACTOR USED ON THIS DRAWING IS 0.75



PAVED SURFACE READINGS

Average	57
Minimum	2.1
Maximum	150
Median	87.03

PROPERTY LINE READINGS

Average	0.4
Minimum	0.0
Maximum	N/A
Median	N/A

\*\* - SPECIFY COLOR

FIXTURE TYPE	SYMBOL	FIXTURE QUANTITY	CONFIG	QUANTITY	VENOZ TOP BRACKETS	QUANTITY	EPA	MOUNTING HEIGHT	POLE TYPE	QUANTITY	WIND LOAD	ALLOWED EPA
ASB-C-400PMH-**-MT @ 0'	+	9	SGL	2	N/A	N/A	2.17	22'	EXISTING	5	90mph	N/A
ASB-C-400PMH-**-MT @ 30'	+	6	SGL	6	N/A	N/A	2.17	22'	EXISTING	6	90mph	N/A

\* PROJECT WIND LOAD CRITERIA BASED ON:  
ASCE 7-10 WIND SPEEDS (3-SEC PEAK GUST MPH)  
50 YEAR MEAN RECURRENCE INTERVAL

THIS LIGHTING PATTERN REPRESENTS ILLUMINATION LEVELS CALCULATED FROM LABORATORY DATA TAKEN UNDER CONTROLLED CONDITIONS IN ACCORDANCE WITH ILLUMINATING ENGINEERING SOCIETY APPROVED METHODS. ACTUAL PERFORMANCE OF ANY MANUFACTURER'S LIGHTING MAY VARY DUE TO VARIATION IN ELECTRICAL VOLTAGE, TOLERANCE IN LAMPS, AND OTHER VARIABLE FIELD CONDITIONS.

UNLESS OTHERWISE SPECIFIED, ALL DIMENSIONS ARE IN INCHES

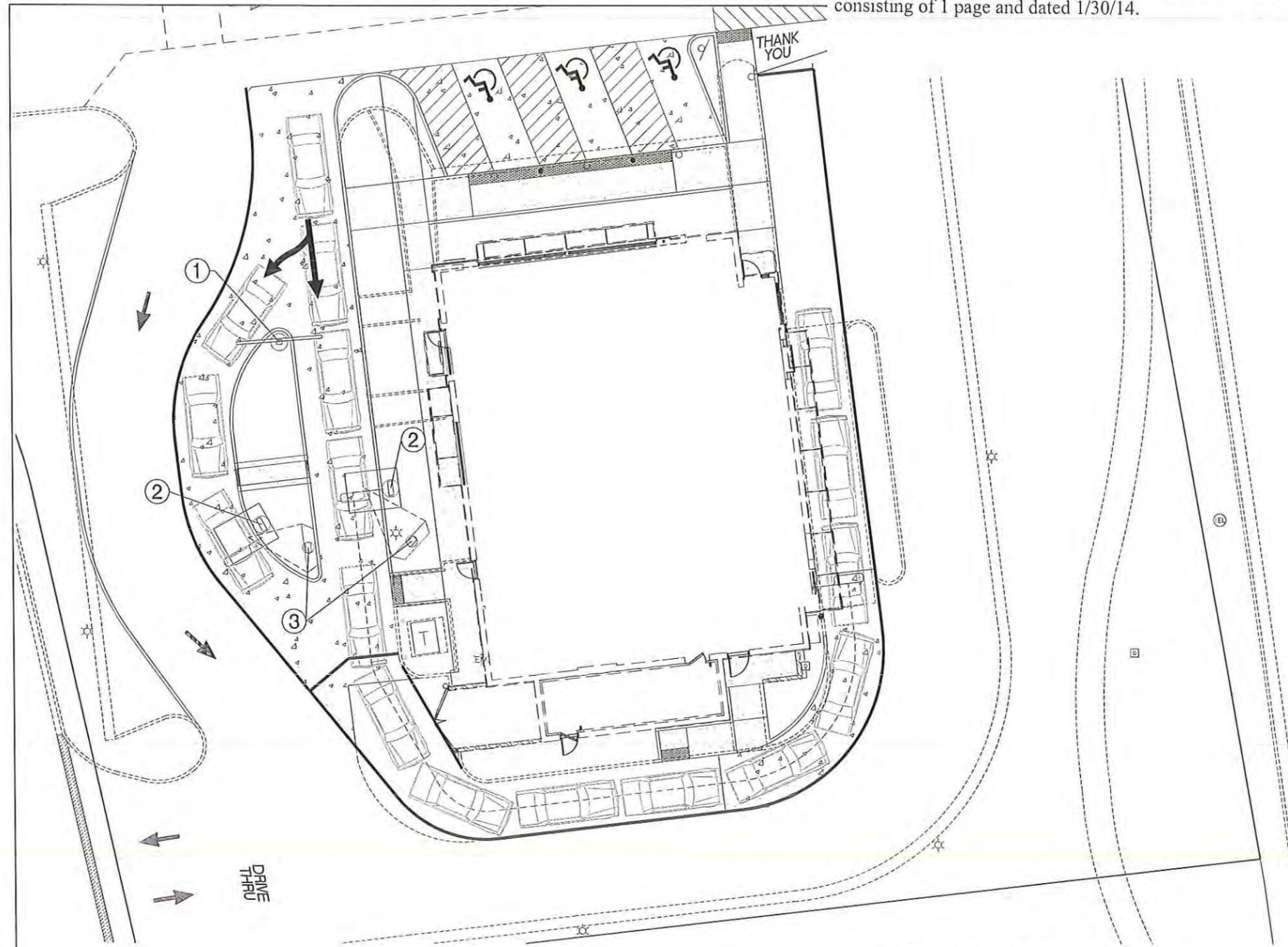
NOTES:

SCALE	1" = 20' 0"	DATE	1/8/14	NEXT ASSEMBLY	REF. DWG.
DRAWN BY	MW				
CHECKED BY					
APPROVED BY					
APPROVED BY					
APP'D ORDER					
PROJECT NO.					

POINT-BY-POINT LIGHTING  
Performance Design Lighting Products  
2000 W. Higgins Dr., Suite 100, Vernon Hills, IL 60061  
1-800-344-0148

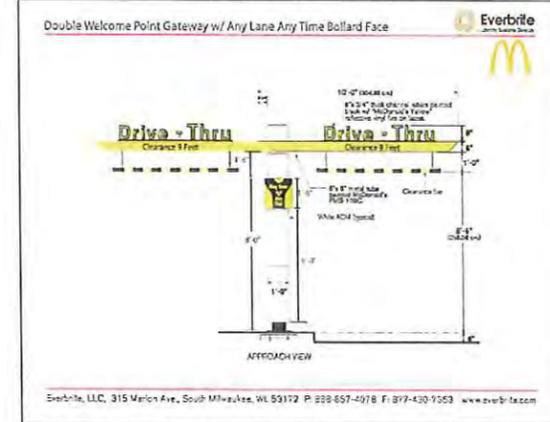
POINT-BY-POINT FOOTCANDLE PLOT FOR  
- McDONALD'S -  
700 N MILWAUKEE AVE  
VERNON HILLS, IL

DRAWING NUMBER  
1PCP28586-2A

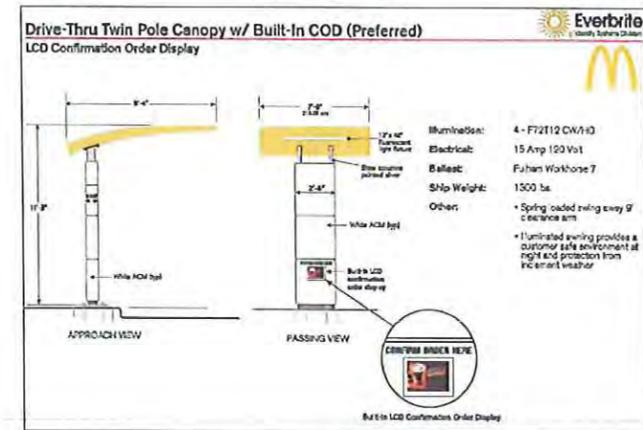


### FREESTANDING SIGN LEGEND

① McDONALD'S DOUBLE WELCOME POINT GATEWAY  
 "DRIVE-THRU" SIGNAGE AREA = 3.61 S.F. sq., "ANY LANE ANY TIME" SIGNAGE = 2.13 S.F.



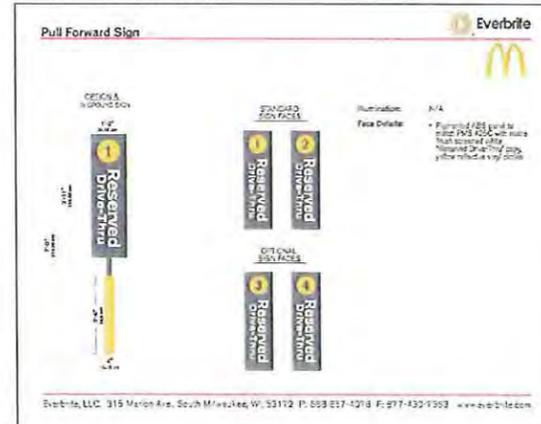
② McDONALD'S DRIVE-THRU TWIN POLE CANOPY W/BUILT IN COD



③ McDONALD'S O.P.O. MENU BOARD  
 SIGNAGE AREA = 31.32 S.F. NET ; 38.81 S.F. GROSS



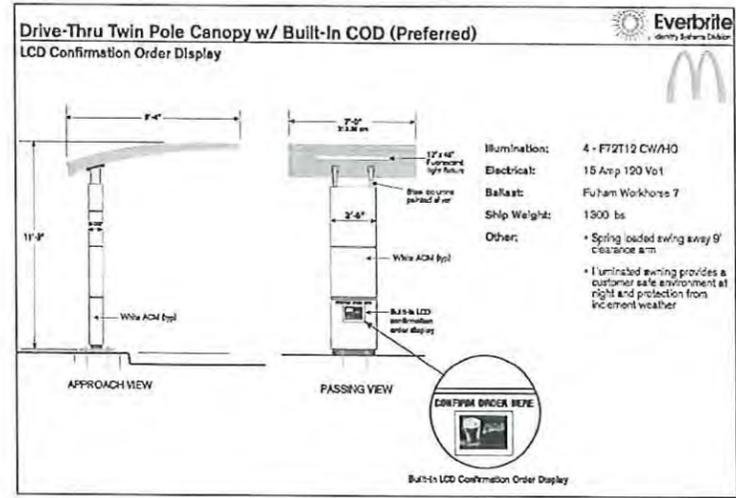
④ McDONALD'S PULL FORWARD SIGNAGE  
 SIGNAGE AREA = 4.57 S.F. sq.



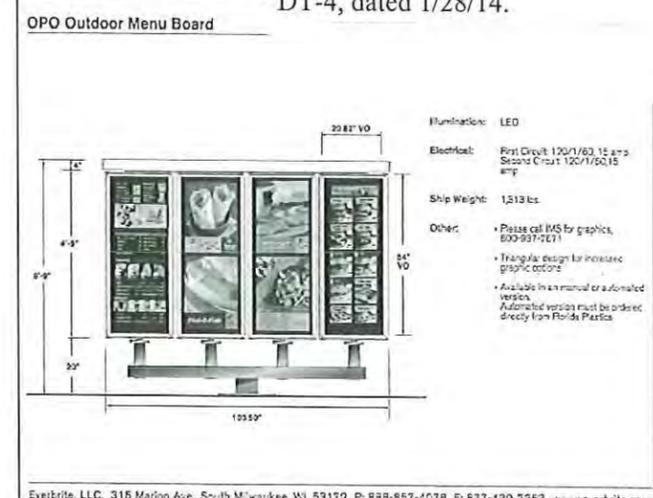
DATE	01/30/14
REVISED X-BASE	01/30/14
REVISED PER VILLAGE REQUEST	
NO.	1-01
Prepared For:	Langle Design Group 158 W. Main Street Lena, IL 61848
Prepared By:	McDonald's - VERNON HILLS, IL 200 N. Milwaukee Avenue Vernon Hills, Illinois
Checked By:	J. MILLER
Design By:	K. SACK
Drawn By:	K. SACK
Date:	MAY 14, 2013
Scale:	1" = 10'
Project No.:	13-010
Page:	1 of 1
Project:	LC #12-1085

Watermark Engineering Resources, LTD  
 2011 Glenn Woods Parkway, South Milwaukee, WI 53122  
 phone 855-375-1000 fax 855-201-8800 www.watermark-engineering.com

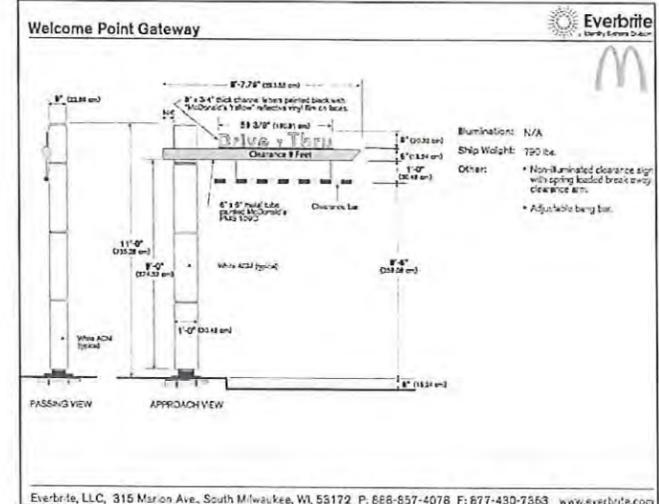
Freestanding Signage Exhibit



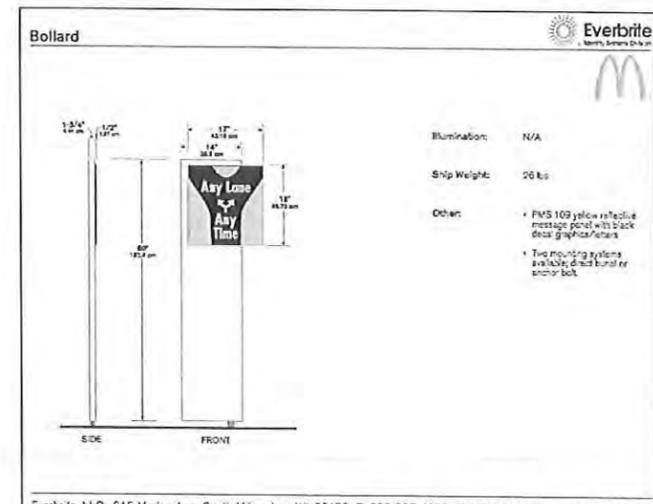
1 DRIVE-THRU TWIN POLE CANOPY W/BUILT-IN COD  
SEE SHEET SSD-1 FOR FOUNDATION DETAIL



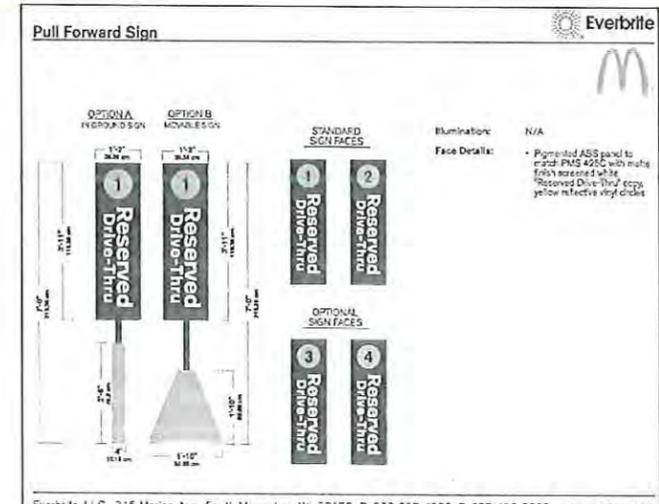
2 OPO MENU BOARD ELEVATIONS  
SEE SHEET SSD-1 FOR FOUNDATION DETAIL



3 SINGLE WELCOME POINT GATEWAY  
SEE SHEET SSD-2 FOR FOUNDATION DETAIL



4 BOLLARD DETAIL



5 PULL FORWARD BOLLARD SIGN DETAIL

NO.	REVISIONS	DATE
1	NO REVISIONS	01/08/14
2	NO REVISIONS	01/28/14

Prepared For:

Single Design Group  
158 W. Main Street  
Lena, IL 61048  
McDonald's - VERNON HILLS, IL  
700 N. Milwaukee Avenue  
Vernon Hills, Illinois

Prepared By:

Watermark Engineering  
RESOURCES LTD  
2631 Greeng Woods Parkway, Suite 100, U. City, IL 60187  
phone 630-375-1000 fax 630-336-0800 www.watermarkengineering.com

CHECKED BY: J. MILLER  
DESIGN BY: M. BAILEY  
DRAWN BY: K. SACK  
DATE: NOVEMBER 21, 2013  
SCALE: NONE  
PROJECT NO.: 13-010

DRIVE-THRU DETAILS

DT-4  
LC #12-1085

DRIVE-THRU DETAILS